

The Hamlet of Egypt Subarea Plan and Guidelines 2003

**A Refinement and Supplement to the
Town of Perinton
Comprehensive Plan Update
2000
Egypt Subarea
Report and Recommendations**



AUGUST 2003



**TOWN OF PERINTON
NEW YORK**

The Hamlet of Egypt Subarea Plan and Guidelines - 2003:

A Refinement and Supplement to the Town of Perinton Comprehensive Plan Update 2000:
Egypt Subarea Report and Recommendations

Issue Date August 12, 2003

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I. INTRODUCTION

A. Objectives

In March of 2003 the Hamlet of Egypt Master Plan Advisory Committee (referred to as the Committee in this document) began a process to refine existing Town documents that specifically address development within the Egypt Subarea. The Committee, consisting of Town officials, board representatives, Hamlet residents, developers and professional design consultants, was formed in reaction to recent development proposals for three specific areas within the Hamlet Center vicinity.

Because each of these development proposals have raised concern with the community, the intent of this refinement was to address the key issues and concerns identified and create a development plan that addresses these issues, is appropriate for the future of the Hamlet, and is consistent with the Town of Perinton Comprehensive Plan.

This document is intended to serve as guidelines that are recommendations of the Committee. It is recommended that the guidelines be applied in the following manner:

- The Hamlet of Egypt Subarea Plan (Section III of this document) applies to all properties within the boundary of the Subarea (including the area recommended for adoption into the Subarea).
- The Hamlet Development Guidelines (Section IV of this document) apply to all properties within the boundary of the Subarea that require Planning Board approval (including the area recommended for adoption into the Subarea).
- The Hamlet Architectural Guidelines (Section V of this document) apply to all properties within the boundary of the Subarea that require Planning Board approval. The Perinton Historic Architecture Commission (HAC) shall review all such development proposals based upon these Guidelines.

B. Egypt Master Plan Advisory Committee 2003

The following is a list of the Egypt Master Plan Advisory Committee 2003:

James Smith	Town of Perinton, Supervisor
Wm. Scott Copp	Town of Perinton, Director Building Department
Thomas Beck	Town of Perinton, Commissioner of Public Works
Carolyn H. Saum	Town of Perinton, Town Board
Ann Parks	Town of Perinton, Historic Architecture Commission Chair
Ken Gordon	Town of Perinton Larsen Engineering
Mark Anderson	Town of Perinton, Planning Board
Jim Brasley	Town of Perinton, Chair, Planning Board
Dwight Paul	Town of Perinton, Planning Board
David Schaeffer	Town of Perinton, Chair, 2000 Master Plan
John Minichiello	Town of Perinton, Conservation Board
Karen Crandall	Town of Perinton, Conservation Board
Kate Fisher	Resident (Mason Area Neighbors)
Robin Sherlock	Resident (Mason Area Neighbors)
Kevin Kupka	Resident (Egypt Hills Neighborhood)
Arlene Herrington	Resident (Egypt Hills Neighborhood)
James Barbato	Pridemark Homes (Developer)
Robert Keiffer	A.D. Longwell (Developer)
Gary B. Olin	Bergmann Associates
Mark R. Johns	Bergmann Associates
Christopher P. Lopez	Bergmann Associates

C. The Hamlet of Egypt Subarea Plan and Guidelines

The result of this Advisory Committee design process is the creation of The Hamlet of Egypt Subarea Plan and Guidelines 2003: A Refinement and Supplement to the Town of Perinton Comprehensive Plan Update 2000 Egypt Subarea Report and Recommendations (2003 ESP). This document will be used by the Town of Perinton to guide appropriate development and architectural aesthetics for the Egypt Subarea.

The document consists of three main parts: the Hamlet of Egypt Subarea Plan (Land Use & Circulation), Hamlet Development Guidelines, and Hamlet Architectural Guidelines.

The Hamlet of Egypt Subarea Plan (Land Use & Circulation) is a graphic refinement of the Concept Plan for Land Use and Circulation that was produced by Larsen Engineers for the Town of Perinton Comprehensive Plan Update 2000 Egypt Subarea Report and Recommendations (2000 ESR). The Plan incorporates issues from both the projected future DOT Route 31 improvements and the design process of the Egypt Master Plan Committee 2003. This plan is intended to be used in conjunction with the Hamlet Development & Architectural Guidelines.

The Hamlet Development Guidelines are standards that have been created to address issues such as land use designations, adjacencies, transition zones, buffers and density. These guidelines are intended to promote appropriate development specific to the Hamlet of Egypt.

The Hamlet Architectural Guidelines are standards that have been created to address specific aesthetic issues within the Egypt Subarea. These guidelines are intended to promote appropriately scaled and detailed buildings, and shall be considered a refinement to the 2000 ESR, Appendix B: Design Guidelines prepared by the Landmark Society.

II. PROCESS

A. Planning Design Process

The planning design process, led by the design consultants, involved the discussion and collaboration of the Committee. A series of meetings were held over the course of four months to discuss the key issues surrounding the future development of the Hamlet. The process was broken into four main phases: Orientation and Key Issues, Schematic Design, Design Development, and Finalization of the Design Results.

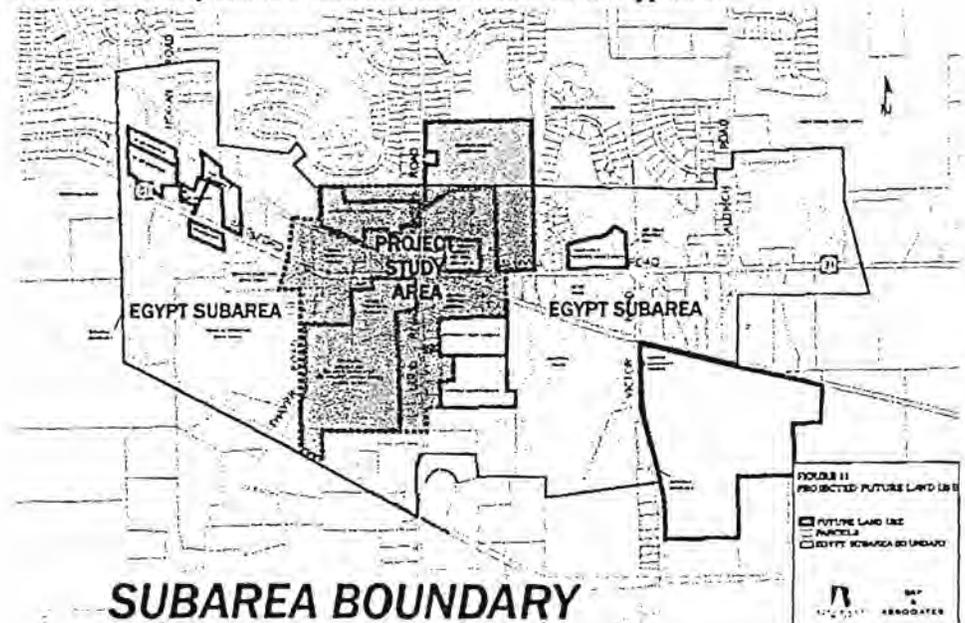
B. Orientation and Key Issues

The Committee began by orienting themselves to the unique features of the Hamlet of Egypt. Documents containing aerial photographs, parcel boundaries, local landmarks, zoning designations, land uses, land and water features, roads and trails, etcetera, were distributed and reviewed. A Project Study Area was also established which specifically focused on the Hamlet Center (intersection of NYS Route 31 and Mason/Loud Roads).

In addition to the review of site data, the Committee created a list of Key Issues that apply to concerns and visions relating to the future development of the Hamlet. These Key Issues were grouped into seven main categories including: Hamlet Identity, Traffic Patterns/Circulation, Historic Character - Preservation and Creation, Economic Vitality

of the Hamlet, Buffering/Land Use/Adjacencies, Density of Existing and New Developments, and Existing Ecology.

A list of these Key Issues is included for reference in the Appendix.



C. Schematic Design

Based upon the initial meeting a schematic design process began that attempted to address the Key Issues identified by the Committee, as well as maintain the established principles and intents of the Town of Perinton Comprehensive Plan Update 2000 (2000 CP) and the 2000 ESP. The following topics were discussed: Key Issue Assessment and Approaches, National and Local Neighborhood Development Precedents, Traffic Capacity Analyses, Concentric Density Gradation, and a Preliminary Land Use Plan.

D. Design Development

The next phase, Design Development, involved the Committee's discussion of schematic design concepts prepared by the design consultants. These concepts were then developed and refined into a land use diagram that was site-specific. This diagram included a higher level of detail and began to incorporate the actual parcel boundaries, existing land features, and significant and historic sites.

In addition to the advancement of the Land Use Diagram, existing established Town land use designations were discussed and proposed land use sub-designations were presented by the design consultants. These sub-designations put forth quantitative site parameters such as density (lot size and units per acre), land use adjacency and transition zones. The sub-designations were then revised based upon Committee discussion.

Also during this phase, conceptual development designs were created to test and illustrate the proposed design standards for Planned Residential development upon specific future development parcels within the Hamlet Center area. These test layouts were performed on the northeast and southwest quadrants of the Hamlet Center.

Once general consensus was achieved by the Committee regarding the Land Use Plan, Hamlet development and architectural guidelines that supplement the existing 2000 ESP Appendix B were developed. These Guidelines were based upon the unique architectural history of the Hamlet as well as the principles of traditional neighborhood design. The Guidelines are intended to work in conjunction with the Hamlet of Egypt Subarea Plan (Land Use & Circulation) in leading appropriate future development.

E. Design Process Results

Through further discussion and refinement the consultants created a draft of the Hamlet of Egypt Subarea (Land Use & Circulation), Hamlet Development Guidelines and Hamlet Architectural Guidelines. These drafts were reviewed by the Committee, as well as the Perinton Historic Architecture Commission, revised and edited to produce this document.

Taken together, in conjunction with the established Town codes, standards, plans and recommendations, this document is designed for the Town's use in assessing and guiding development proposals. Created through a committee process, these recommendations are intended to lead the Hamlet of Egypt towards the achievement of the local community's goals.

III. THE HAMLET OF EGYPT SUBAREA PLAN

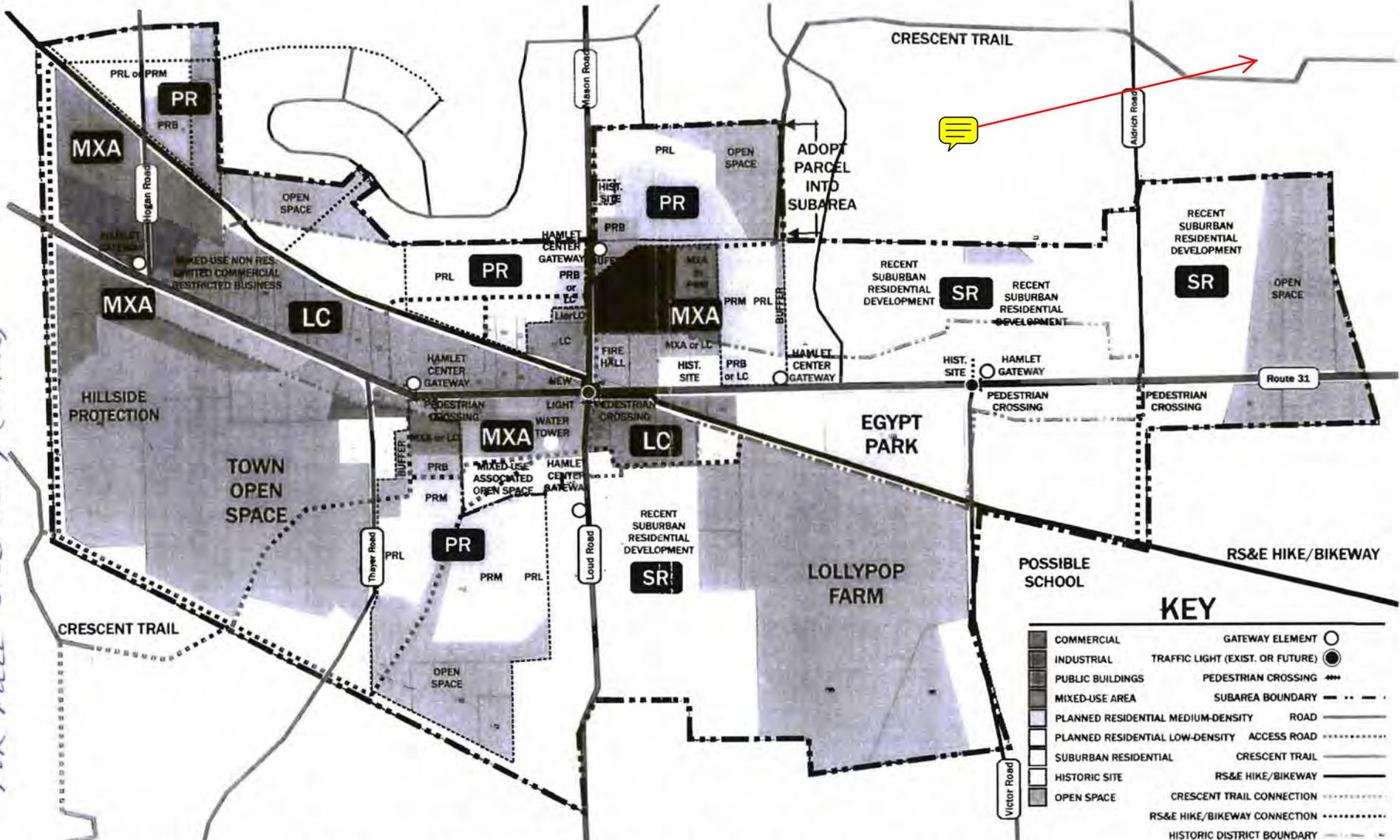
The Hamlet of Egypt Subarea Plan - Land Use & Circulation is a refinement of the map created for the Town of Perinton Comprehensive Plan Update 2000 - Egypt Subarea Report and Recommendations. The Plan identifies proposed land use and circulation patterns for the Egypt Subarea. Like its predecessor, this Plan provides recommendations for the future development of the Subarea. It is important to note that this is a conceptual plan, it is not a zoning map, and the Plan does not define specific boundaries. However, the general proportions of the depicted land use is recommended.

The refined plan takes into account the anticipated Route 31 NYDOT improvements and provides greater detail to previously defined land use designations (particularly the Planned Residential designation).

In addition, the Egypt Historic District boundary has been overlaid and suggested Hamlet gateway locations are identified on the Plan.

As indicated on the plan, it is recommended that the area in the northeast quadrant, west of Mason Road (adjacent to the historic cemetery) be adopted into the Subarea boundary. This area is part of a parcel that extends down to Route 31 and currently is situated so that it is both inside and outside of the Subarea boundary line.

SEE "Hamlet of Egypt..." Appendix A-2
 FOR FULL SIZE COPY (color)



KEY

	COMMERCIAL		GATEWAY ELEMENT
	INDUSTRIAL		TRAFFIC LIGHT (EXIST. OR FUTURE)
	PUBLIC BUILDINGS		PEDESTRIAN CROSSING
	MIXED-USE AREA		SUBAREA BOUNDARY
	PLANNED RESIDENTIAL MEDIUM-DENSITY		ROAD
	PLANNED RESIDENTIAL LOW-DENSITY		ACCESS ROAD
	SUBURBAN RESIDENTIAL		CRESCENT TRAIL
	HISTORIC SITE		RS&E HIKE/BIKEWAY
	OPEN SPACE		CRESCENT TRAIL CONNECTION
			RS&E HIKE/BIKEWAY CONNECTION
			HISTORIC DISTRICT BOUNDARY

THE HAMLET OF EGYPT SUBAREA PLAN 2003 LAND USE & CIRCULATION

HAMLET OF EGYPT TOWN OF PERINTON NEW YORK



IV. HAMLET DEVELOPMENT GUIDELINES

A. Introduction

The Hamlet Development Guidelines are standards that have been created to address issues such as land use designations, adjacencies, transition zones, buffers and density. These guidelines are intended to promote appropriate development specific to the character and setting of the Hamlet.

B. Phasing Requirements

Due to the existing and anticipated future traffic demands, various transportation studies have been performed upon the Egypt Subarea and Route 31 Corridor (see 2001 ETS for detail). Traffic calculations that took into account the development of the Subarea and the surrounding areas were used to inform and develop the DOT Route 31 improvements. However, until the Route 31 improvements in this area have been completed, the Committee recommends that a limit be placed upon the development that can occur within the Subarea. The following formulas, derived by SRF & Associates, are to be used to calculate the maximum allowable buildout for two specific areas, north of 31 on Mason Road (includes the Pridemark and Collier properties), and south of 31 along Loud Road (includes the Longwell property). No units may be built beyond the limits set forth by these formulas until the realignment and signalization of the Mason/Loud and Route 31 intersection are completed.

- **Allowable Buildout North on Mason:**

$$\text{Mason Rd Buildout} = [0.75(\text{SFU}) + 0.55(\text{APTS}) + 0.45(\text{TH/C})]0.8 \leq 30$$

SFU = single family unit; APTS = apartments; TH/C = condominiums
30 = maximum left turn vehicles per PM peak hour

- **Allowable Buildout South along Loud:**

$$\text{Loud Rd Buildout} = [0.65(\text{SFU}) + 0.46(\text{APTS}) + 0.46(\text{TH/C})]0.8 \leq 32$$

SFU = single family unit; APTS = apartments; TH/C = condominium/townhouse
32 = maximum left turn vehicles per PM peak hour

Any combination of residential land uses that satisfy these equations may be permitted. The following scenarios are provided as examples:

- **Allowable Buildout North on Mason Scenarios:**

50 single family units builtout only = 30 (turns)
68 apartments builtout only = 29.92 (turns)
83 condominium/townhouses only = 29.88 (turns)
15 SFU, 22 APTS & 30 TH/C = 29.48 (turns)

- **Allowable Buildout South along Loud Scenarios:**

62 single family units builtout only = 31.72 (turns)
86 apartments builtout only = 31.65 (turns)
86 condominium/townhouses only = 31.65 (turns)
12 SFU, 42 APTS & 28 TH/C = 32 (turns)

C. Hamlet Character

The Hamlet of Egypt has a rich agricultural history. It is strongly encouraged that developers interested in the Hamlet become familiar with Egypt's history by referring to "Identification of Architectural and Visual Elements that Define the Historic Character of the Hamlet of Egypt, Town of Perinton," prepared by Ann B. Parks and the Landmark Society of Western New York (1999 LSS). Excerpted design guidelines from this document were included in the 2000 ESR. The historical character of the Hamlet should be maintained through any new development or renovation project within the Subarea.

Rural towns typically have two primary development types: rural countryside and hamlet centers. The hamlets are denser and contain a greater variety of uses. The limits of a hamlet center are clearly defined by its transition to the more rural and less dense periphery. This concept applies to Egypt in that a concentric density gradation (from dense to less dense), from the intersection of Pittsford-Palmyra Road (Route 31) and Mason/Loud Roads has occurred and has been determined to continue to be appropriate and desired. It has also been determined that, due to the presence of Route 31 and its heavy traffic volume, the density transition east and west from the Hamlet Center will be more gradual than that north and south (along Mason/Load Roads) from the Hamlet Center. This presents a more linear development pattern. Ultimately, the composite of the concentric and linear patterns result in a 'modified elliptical' density gradation model (see Density Gradation Diagram in Appendix).

Also important to the hamlet character is the identification of existing land and architectural features. These unique features are landmarks or icons which identify the Hamlet of Egypt. New development should serve to enhance the existing integrity of the hamlet character by respecting historic (and non-historic) landmark buildings and structures, land and water features, as well as important vistas. Existing vistas should be maintained or improved through the preservation of existing tree stands, minimization of parking lot size, by using tree-planted parking islands in lieu of paint striping, by the respectful scale and orientation of buildings, etcetera.

D. Density

The issue of appropriate density levels for the Hamlet of Egypt was discussed extensively throughout the committee process. As described in the Hamlet Character section, (and further defined in the Proposed Land Use Designation section) the concept of Density Gradation from the Hamlet Center and from Route 31 out to the periphery of the Subarea is an overriding principle. Although the Committee is recommending a 'modified elliptical' gradation pattern, the maximum allowable density levels of each particular parcel should still be governed by the current zoning designation. Although not specifically endorsed by the Committee, it has been suggested by the Mason Area Neighbors that a density limit be established. Such a density limit would allow the development of an adjacent parcel to increase in density a maximum of two times the density of the existing development. For example, in the case of a new Planned Residential Development adjacent to an existing Suburban Residential Development of 1.5 units per acre, the outer ring of the PR (or PRL) would be limited to 3 units per acre, the next inner ring (or PRM) would be limited to 6 units per acre.

In general, it is the Committee's recommendation that the Planning Board consider two main principals associated with density:

- When considering the Hamlet as a whole, the philosophy of the Density Gradation Diagram should be referenced.

- And when considering specific new developments, the density levels of the existing adjacent parcels should be respected so as to not create drastic contrasts in density.

E. Design Principles

The following is a list of development design principles that describe the vision and goals for development within the Subarea. These principles (except for numbers 21-23) have been excerpted directly from the 2000 Town of Perinton Comprehensive Plan's recommendations for the Hamlet of Egypt.

1. The Hamlet of Egypt should be considered a Town sub-center and, as such, an area where denser development should be encouraged, consistent with the objectives of compatibility with adjacent land uses, limiting natural factors and continued enhancement of the attractiveness of the community.
2. The density and intensity of development shall be greatest closer to the four corners in Egypt (Loud, Mason, Rt. 31) and become less dense more distant from this center. The gradation in density should also be based upon the availability of sewer and water, topography, access opportunities, safety and capacity of the road system, and compatible with surrounding land uses.
3. For residential development, higher density development is appropriate near the core, grading outward to traditional suburban densities, and in the east and south to rural densities.
4. Adequate buffers and/or land use transitions are needed between new development and low density rural residential areas to the south, and developed subdivisions to the north and northeast.
5. Industrial park expansion near the four corners shall be discouraged and consideration be given to change the zoning to limited commercial, planned residential, restricted business, and planned mixed-use.
6. Areas currently zoned Residential B, not considered suitable for residential development, should be rezoned in accordance with recommendations of this plan (refers to 2000 Town of Perinton Comprehensive Plan).
7. Due to topographic constraints, the zoning on the south side of Route 31 west of Thayer Road shall remain flexible, to allow for either commercial, restricted business, or mixed density residential. This is designed to minimize disturbance of the hillside, provide adequate off-street parking, and provide access to adjoining developable property.
8. Community (neighborhood) service and convenience businesses are preferred over businesses primarily upon drawing regional traffic.
9. The Comstock Building is one of the key redevelopment sites in the Hamlet. A combination of zoning restrictions and development incentives should be initiated to both encourage the owners to change the use and improve the looks of the building and grounds.
10. The steep slopes south of Route 31 help define the boundaries and character of the Hamlet and, as such, they shall be protected from encroachment.

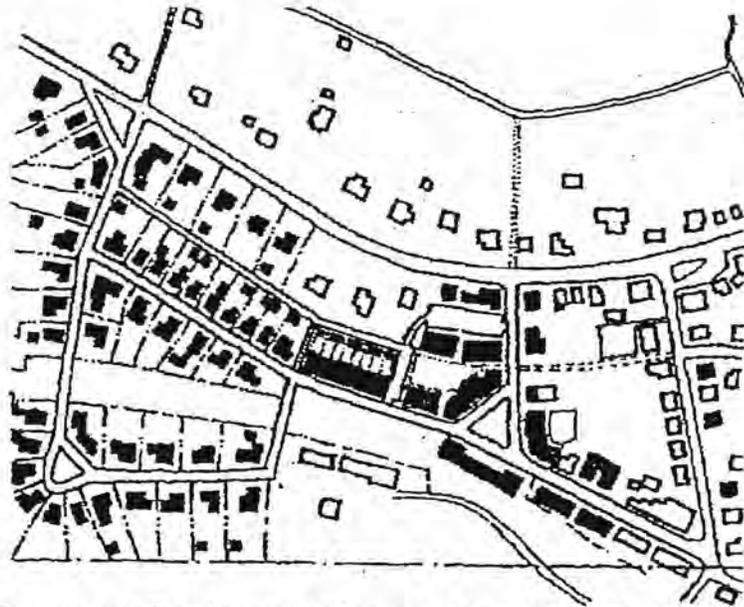
11. Encourage the curbing, paving, and landscaping of parking areas within the subarea, particularly along the commercial strip on Route 31.
12. Subdivisions and other types of development on the periphery of Egypt shall be designed so that easy access can be provided to the Hamlet both for vehicles and pedestrians.
13. A street landscaping plan shall be developed to provide visual and functional continuity within the Hamlet of Egypt.
14. Development plans along Route 31 shall be reviewed with setbacks based upon design guidelines and access management recommendations. Developers may be required to provide right-of-way adjacent to their property, in accord with a the DOT highway construction plan.
15. Investigate the potential interest of property and business owners in a special district for streetscapè improvement.
16. The Planning Board, Historic Architecture Commission, and/or Town Historian should work with property owners to choose appropriate building materials, forms, and architectural details during redevelopment and construction of new buildings in the Hamlet area to reinforce the Hamlet's character and identity.
17. A sense of place can be reinforced with gateway signing, consistent roadside landscaping and design, banners, and other consistent architectural treatments.
18. The Town or Special Improvement District should provide additional parking, landscaping, lighting, street furniture, and other features necessary top support pedestrian interest and use of the Hamlet.
19. The Packard-Wilson (Nelson) Store at the corner of Loud Road and Route 31 has historic value. It is recognized that little can be done to improve roadway safety or access with the building structure in its current location. Relocation is preferable to demolition.
20. Improvements to Egypt should be principally funded by new development, state agencies, and grants rather than with Town funds.
21. The Subarea development is to be rooted in preserving and building upon the historic character of the Hamlet of Egypt.
22. When new buildings are considered, architectural compatibility rather than conformity should be the rule. When existing historic buildings are renovated, preservation based upon the Town's established processes should be the rule.
23. Density shall be appropriate to promote the economic vitality of the Hamlet, but not to the detriment of the historic character.

The Hamlet Development Guidelines set forth in this document are intended to support and give form to these Principles. If a question should arise regarding the specific implementation of the Hamlet Development Guidelines or Hamlet Architectural Guidelines, the Design Principles should be referenced to ensure that decisions are made in keeping with the Subarea vision.

F. Hamlet Development Guidelines

1. Pattern of Development

The pattern of new development should follow the traditional form of a hamlet; more dense closer to the center and less dense at the periphery. In the case of Egypt the density gradation should follow a modified elliptical form (refer to the Density Gradation Diagram in the Appendix). New development near the hamlet center should be designed with small lots, mixed-uses, and interconnected streets and paths to promote close-knit, community-based, and walkable neighborhoods. Further from the hamlet center, development should be less dense, with larger lots and less variation of land use.



Pattern of Development Diagram: More Appropriate Hamlet Development Pattern.



Pattern of Development Diagram: Less appropriate Hamlet Development Pattern.

Development Pattern Diagrams excerpted from the Dutchess County Department of Planning and Development Hamlet Design Guidelines, October 1994.

2. Proposed Land Use Plan, Adjacencies, Transition Zones, Buffers

The following documents should be referred to as guidelines for new development within the Egypt Subarea (document abbreviation in italics):

- Town of Perinton Comprehensive Plan – 2000 (2000 CP)
- Town of Perinton Comprehensive Plan Update - 2000: Egypt Subarea Report and Recommendations (2000 ESR)
- Town of Perinton Comprehensive Plan Update – 2000: Guidelines for Environmental Protection and Growth Management (2000 EPG)
- The Hamlet of Egypt Subarea Plan – 2003 Land Use and Circulation (2000 ESP)
- NYS Route 31/Hamlet of Egypt Transportation Study 2001, Town of Perinton (2001 ETS)

a. Land Use

In general more dense and public-oriented development shall occur closer to the Hamlet Center, and closer to NYS Route 31. (Refer to the Density Gradation Diagram in the Appendix)

Starting from the first core ring, the following land use designations are more appropriate: Limited Commercial, Mixed-Use (non-residential), Restricted Business and Public Buildings.

In the next core ring the following land use designations are appropriate: smaller scale Limited Commercial and Restricted Business, Mixed-Use (with residential) and Planned Residential (includes Planned Residential Medium Density, Planned Residential Low Density and Plan Residential Business/Amenity).

The following outer ring land uses are more appropriate: Suburban Residential, Low Density (Rural) Residential, and Reserved Open Space/Park.

b. Adjacencies

It is important for new development to be respectful of adjacent land use. Many factors should be considered including: appropriate and compatible land uses, density levels, visual and noise considerations, and circulation systems.

The two primary ways of addressing adjacency issues shall be by providing Transition Zones and Buffers.

c. Transition Zones

Transition Zones are areas where an intermediate land use is used to create a more appropriate progression between two less similar land uses. In general, transition zones can help bridge from public to semi-public, semi-public to semi-private, and semi-private to private types of land uses.

The following are examples of Transition Zone scenarios as applicable to the Project Study Area of the Hamlet Center:

- Restricted Business – can be used to transition from Commercial to Residential areas.
- Planned Residential Business/Amenity can be used to transition from Business or Commercial to Residential areas.
- Planned Medium-Density Residential can be used to transition from Business or Commercial to Planned Lower Density Residential areas.
- Mixed-Use (with Residential) can be used to transition from Commercial to Planned Residential.
- Planned Low-Density Residential can be used to transition from higher density Planned Residential to Suburban Residential.
- Suburban Residential can be used to transition from higher density Residential to Rural Areas and Reserved Open Space.

d. Buffers

Buffers are dedicated areas that serve to screen views and/or noise between one property and another. They can be achieved with landscaping elements such as trees, vegetation and land forms, or through hardscaping elements such as walls or fences. Ideally, existing land features (wetlands, woodlots, steep slope areas, streams, etc.) should be identified, preserved and used as buffers. Another effective means of buffering for consideration is to apply minimum setback distances that create build-to lines.

Buffers are recommended where there are specific adjacency concerns and where the use of Transition Zones is not feasible or appropriate.

3. Proposed Land Use Designations (Sub-Designations)

Land Use Designations for the Town of Perinton have been defined in the 2000 CP. These Land Use Definitions, along with Zoning Designations, provide more detail on lot sizes, density levels, and other descriptive information pertaining to land use.

Sub-Designations specific to Egypt have been formulated that are intended to refine some of the more general Designations. The Committee focused primarily upon the Planned Residential Land Use Designation for this document. The following are proposed Land Use Sub-Designations for the Egypt Subarea:

Planned Residential is an existing land use designation defined as follows:

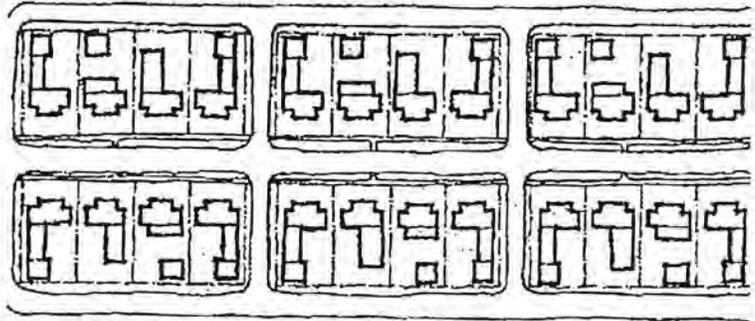
Planned Residential Designation (PR)

These are areas planned as whole, often with combinations of housing styles and densities and/or the incorporation of recreational amenities. Parts of the development could be zoned differently to accommodate the type of development within it, or the project could be approved using Section 278 of Town Law.

The following are proposed sub-designations that would define land use within the Planned Residential designation:

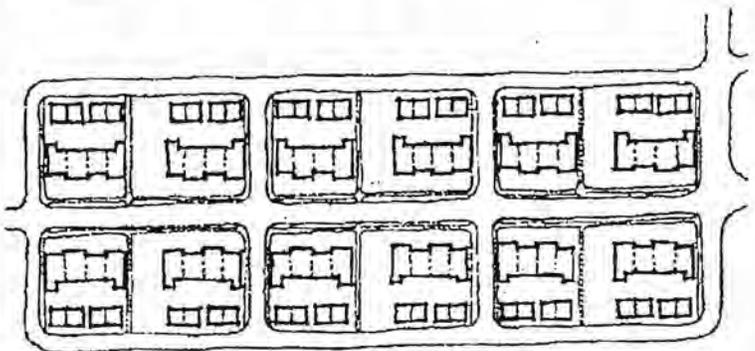
- **Planned Residential Low Density Sub-Designation (PRL)**
A Sub-Designation of Planned Residential for areas that primarily serve as transitions from Suburban Residential to areas of greater density. Housing densities of 3-6 units per acre on lot sizes ranging typically from 7,000 to 11,000 square feet. Single-family houses are strongly encouraged. Duplex units are not encouraged, but if proposed they

must not be predominant, must meet the Hamlet Architectural Guidelines, must be substantially buffered to any existing adjacent single-family land use areas, and must express variety in their architectural treatment. These areas are intended to be oriented in a village-like configuration and linked to the neighborhood and local amenities by a comprehensive pedestrian circulation system. The house footprints are intended to be oriented towards the front yard so that the front facade contributes to the pedestrian character of the street. Rear yard vehicular access is encouraged, but not required.



Planned Residential Low-Density Diagram: Concept Sketch

- Planned Residential Medium Density Sub Designation (PRM)**
 A Sub-Designation of Planned Residential for areas that primarily serve as transitions from Planned Residential Low Density to areas of greater density and/or business, commercial or mixed use areas. Housing densities of 6 to 8 units, single or multi-family, per acre. These areas are intended to be oriented in a village-like configuration and linked to the neighborhood and local amenities by a comprehensive pedestrian circulation plan. Parking ratios are intended to be limited to 1-2 spaces per unit. Rear yard vehicle access is encouraged, but not required. Townhouses and Condominiums are encouraged, apartments are acceptable.



Planned Residential Medium-Density Diagram: Concept Sketch

- Planned Residential Business/Amenity Sub-Designation (PRB)**
 A Sub-Designation of Planned Residential for areas that primarily serve as transitions from Planned Residential areas to areas of greater density and/or business, commercial or mixed use areas. These areas are

intended for resident oriented amenities and restricted services such as parks, clubhouses, recreation facilities, etc. The scale, proportion, and aesthetics shall be compatible with both the adjacent residential and commercial areas. These areas are intended to be linked to the residential areas and local trail systems by a comprehensive pedestrian circulation plan.

Mixed Use Area is an existing land use designation defined in the 2000 ESP as follows:

Mixed Use Designation (MXA)

These are areas where commercial, office and residential uses could intermix, either in separate parts of the development, separate buildings, or mixed uses in the same building. These could be new developments or redeveloped properties. They would be planned with a unifying theme or style appropriate for the specific area being developed or redeveloped, and could include amenities for tenants and visitors/shoppers. Mixed use areas could have both planned and single use projects. There currently is not a corresponding zoning category, but similar development patterns have occurred in several areas of Town.

4. Landmarks and Natural Features

The Hamlet of Egypt has inherent character based upon its history and natural beauty. New developments shall be respectful of this character and shall identify, integrate and enhance any existing landmarks or natural features on or adjacent to their property. New developments should also identify and respect any existing view-sheds or sight-lines from and to existing adjacent properties by mitigating negative visual or audio impacts (i.e. light pollution, noise pollution, obstruction of views due to poor building orientation, etcetera). Developers shall recognize LDD (Limited Development District) conditions, visual elements as identified by the Landmark Society, as well as other local iconographical elements when planning developments. Whenever possible, public buildings, landmark buildings, open space, parks and other focal points should be preserved and located at primary intersections and/or prominent sites.

The 2000 EPG should be referred to for environmental guidance when developing within the Subarea.

5. Circulation Patterns

Recommendations in regards to circulation patterns have been made in both the 2000 ESP and the 2000 ETS. The 2000 ESP recommends that, "commercial and higher density development should be accessed directly from Route 31, and residential traffic directed to Loud, Mason, Aldrich, Hogan or Thayer Roads." Although the future Route 31 improvements do not directly align with the circulation plan suggested in the 2000 ESAP, efforts should be made to uphold this recommendation.

In general, new streets should be interconnected in cohesive and straight-forward patterns; they should be laid out so as to mitigate safety issues; they should connect to existing streets whenever possible; and they should be compatible with the Subarea circulation plan. Dead end streets are discouraged (unless dictated by topography).

When conflicting issues arise in a specific street circulation decision-making situation, it is recommended that the alternative that promotes greater traffic safety should prevail.

However, it should be recognized that, although interconnecting streets in principle are more consistent with traditional hamlet patterns and provide safer traffic results, there is a genuine concern of the local residents about an increase of traffic on neighborhood streets. Creative solutions such as limited access, traffic calming measures and strategic one-way traffic patterns should be considered on a case by case basis in sensitive residential areas.

Sidewalks and trails should be integral with the street layout of new developments. Pedestrian circulation shall be provided such that residential areas are linked to the Hamlet Center commercial core, public parks and buildings as well as other existing trail systems.

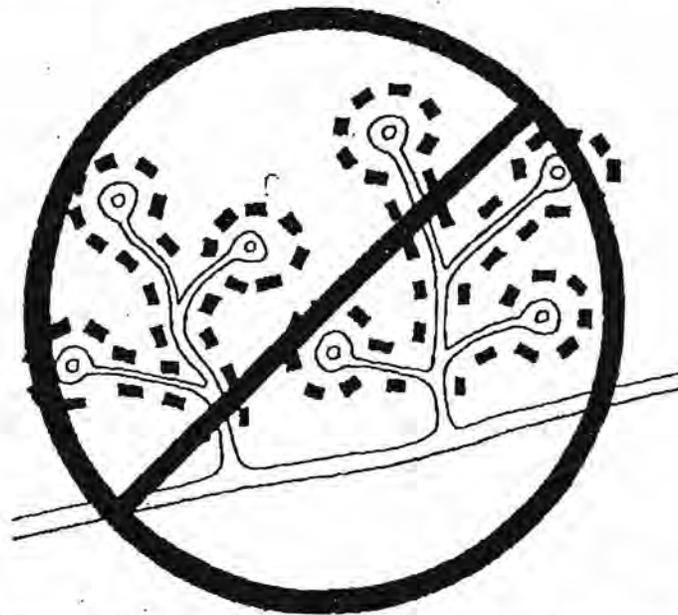
6. Street and Road Access

Street and road access should be consistent with 2003 ESP, 2001 ETS and the planned future NYSDOT improvements for Route 31. Access shall be provided in accordance with the Access Management Guidelines described in the 2001 ETS, Section IV, C, 4.

Because, NYDOT will limit (and in some areas) not allow new access to and from Route 31, it is recommended that access and traffic be directed to the collector streets and minor arterial roads, and away from neighborhood streets.



Circulation Pattern Diagram: More appropriate hamlet circulation pattern of inter-connecting streets.

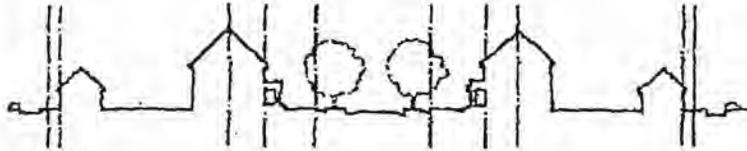


Circulation Pattern Diagram: Less appropriate hamlet circulation pattern of cul-de-sac configurations that do not connect.

Circulation Pattern Diagrams excerpted from the Dutchess County Department of Planning and Development Hamlet Design Guidelines, October 1994.

7. Street Dimensions

Streets within the Subarea (excluding major arterials) should be as narrow as possible while still meeting the Town requirements, accommodating cars, service and emergency vehicles. Streets should be designed for slow speeds with traffic calming measures in mind (refer to 2001 ETS, Section IV, C, 3).

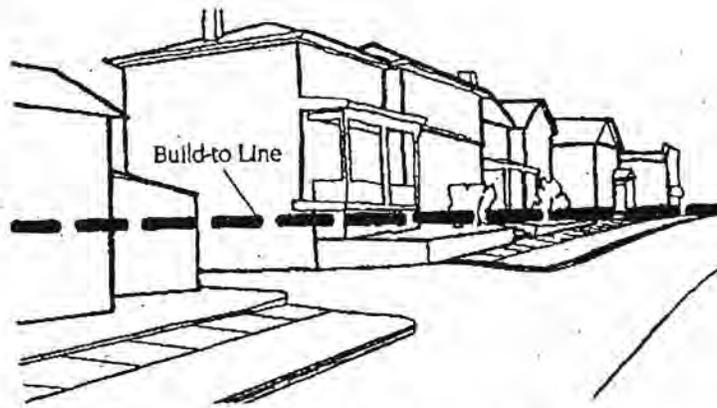


Street Section Diagram: More appropriate hamlet street section with narrow tree-lined streets and defined public and private zones.

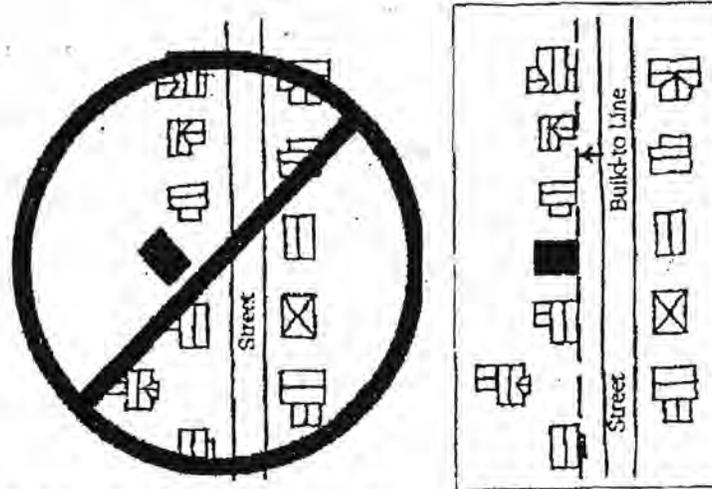
Sidewalks should be provided on at least one side of the street to promote pedestrian circulation. Sidewalks shall be ADA accessible where practical.

8. Building Orientation and Setbacks

Consistent front setbacks along streets are strongly encouraged. New buildings on an existing street shall relate to the established front setback line. Front building facades should be parallel to the street. The front setbacks shall generally decrease the closer the building is to the Hamlet Center. Buildings located in the rural periphery should have greater front setbacks (however, there is precedent for some agrarian buildings to be oriented closer to the street). Refer to the following diagrams for typical residential building orientation.



Building Orientation Diagram: More appropriate building orientation where buildings are aligned along a consistent build-to front setback line.



Building Orientation Diagram: More appropriate building orientation on the right, less appropriate on the left.

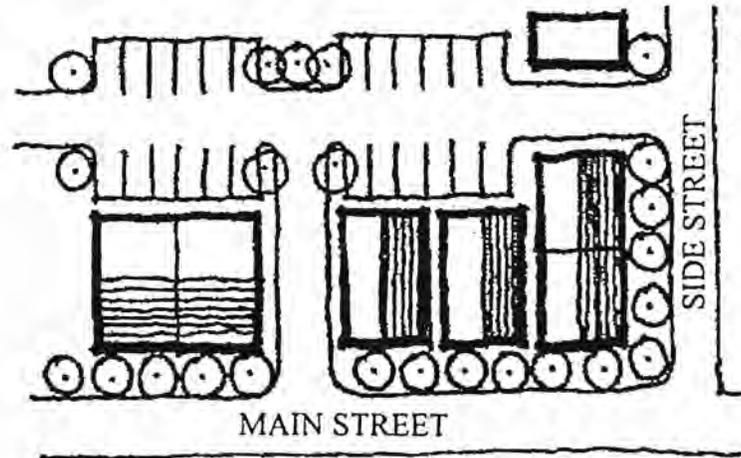
Building Orientation Diagrams excerpted from the Dutchess County Department of Planning and Development Hamlet Design Guidelines, October 1994.

9. Lot Sizes and Dimensions

Lot sizes shall conform to Town Zoning Code, land use designations and density requirements. Minor variation in lot size is encouraged (in keeping with traditional hamlet patterns). Typical residential lot frontages should be between 40 and 80 feet. In general residential lot sizes should be smaller closer to the Hamlet Center and larger further from the Hamlet Center.

10. Parking

Small parking lots within the Subarea should be oriented to the rear of the buildings. If this is not possible based upon the lot configuration, parking should be oriented to the side and screened from the street (see Buffers). Corner parking lots are strongly discouraged. If corner parking lots are unavoidable, screening devices should be used to define the corner edge (i.e. row of trees, planters, fencing, etc.).

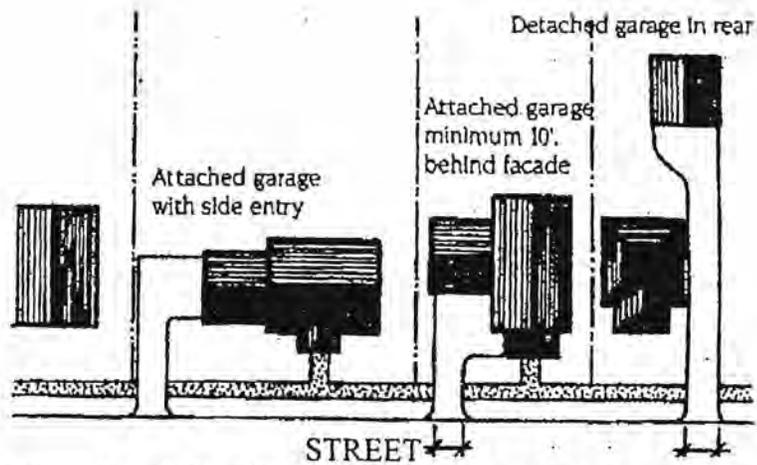


Parking Diagram: More appropriate parking scenario with shared parking lots located behind buildings linked by an access road.

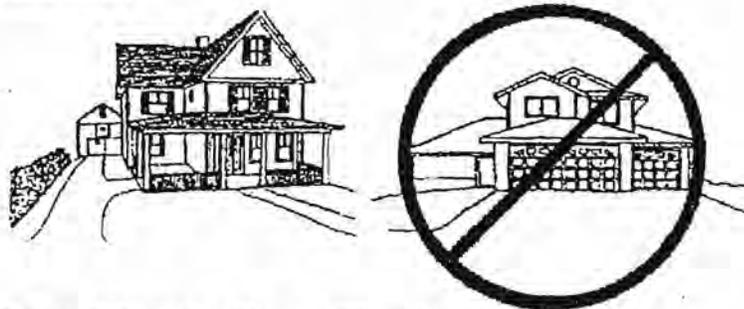
Parking lots should contain no more than 85% the capacity of the anticipated peak demand. To compensate for peak parking demand, rear lots of adjacent properties shall be adjoined by alleys wherever possible to promote shared parking. Planter islands should be used in lieu of paint-striped islands in parking lots.

Parking lot designs are encouraged to integrate connections to surrounding pedestrian trail and sidewalk systems. It is recommended that public parking areas within the Hamlet Center be considered that are strategically placed to accommodate local commercial and public amenity access.

For residential parking, detached or attached garages oriented to the rear of the house are recommended. Garages shall be located so that cars parked outside do not project beyond the front building façade. Where feasible, rear access alleys are encouraged so as to promote pedestrian activity at the street. Curb cuts for residential driveways should not exceed 12' in width. For multi-family housing, the use of garages is encouraged in lieu of large parking lots.



Garage Orientation Diagram (Plan): More appropriate garage orientation scenarios (attached or detached) set back from the front house facade.



Garage Orientation Diagram (Elevation): More appropriate garage orientation on left with garage located in the rear. Less appropriate garage orientation on right where garage doors dominate the building.

Parking and Garage Orientation Diagrams excerpted from the Dutchess County Department of Planning and Development Hamlet Design Guidelines, October 1994.

11. Streetscaping

It is anticipated that developers of commercial and planned residential parcels will incorporate appropriate types and amounts of pedestrian paving, landscape planting, site accessories, signage and site lighting to achieve attractive on-site pedestrian environments. It is further anticipated that enough design components will be incorporated into the design, from the following, to achieve a "pedestrian" character intended for developments in this Subarea:

a. Signage

It is recommended that a new signage system for pedestrians and motorists be implemented for the Subarea. The signage system should be coordinated to promote the larger goals of these design guidelines and the community vision of this rural Hamlet. Appropriate materials for the signage system include timber, rough-hewn stone or cobbles, and steel. The system should be developed as a hierarchical system regarding design, content, and location of the signs and circulation goals for the Subarea. Attention should be given to materials, scale, graphics, orientation, color, lighting, and other physical design issues to coordinate with other streetscape and

architectural elements of the Hamlet. Where possible, signs should be consolidated to eliminate or reduce visual clutter.

b. Street Lighting

A new consistent system of street lighting should be installed in the Route 31 corridor. Functionally, the addition of street lighting improves nighttime visibility and increases safety for motorists and pedestrians. However, consideration should be given to the aesthetic qualities of the lighting units and emitted light both during the day and nighttime hours. Pedestrian scale lighting should be included in a hierarchical system of streetlights. Additionally, nodal spaces along the corridor, like a trailhead, public park, or street intersection, should be addressed in the lighting system.

c. Street Furniture

Throughout Egypt, a coordinated system of street furniture should also be planned. Street furniture includes items such as benches, chairs, tables, umbrellas, garbage receptacles, tree grates, tree guards, bollards, bicycle racks and kiosks. Street furniture should be placed based on projected need or to emphasize a function of a specific place along the corridor. For example, amenities at a public bus stop or at a public park are recommended. The street furniture system should be planned in concert with all other recommended guidelines to best unify all elements of the Hamlet in terms of design, materials, color, scale, and location. Appropriate materials are timber, rough-hewn stone, and steel.

d. Road Pavement Features

It is recommended that granite curbing be utilized in the Hamlet Center. A curbed road would aid traffic calming and emphasize the community setting of the Hamlet as opposed to its more rural non-curbed periphery.

Special pavement types could be used in pedestrian crossing areas, two-way left-turn lanes, entrance thresholds, medians, and bicycle lanes, to provide further identity to the Hamlet and offer traffic calming benefits. Paving materials with contrasting colors and textures could be added to these areas or substituted for typical road asphalt. These materials could consist of cobbled stone paving to emphasize the rural character of the area, or could be a more cost-effective solution, like stamped and colored concrete to mimic natural stone cobbles. The use of pavement alternatives reduces the visual dominance of the asphalt roadway and offers the opportunity to coordinate with materials used in other places in the Hamlet, strengthening the identity of Egypt.

As a historical reference to the historic RS&E Trolley Line, a change in pavement type could mark the former location of the functional rails. New, non-functional flush rails and ties could be installed at the former location. The rails could serve an educational role as an interpretive element of the former significance and location of the railroad and a unique feature for the RS&E Hikeaway-Bikeway.

e. Pedestrian Sidewalk Features

Sidewalks should be of five foot wide concrete construction and should ideally occur along both sides of Route 31, offset a minimum of five feet from the curb line. Wider sidewalks in front of storefronts shall be considered when seating areas are incorporated into the design.

Beyond the Hamlet Center, sidewalks could be of asphalt construction and should maintain a consistent width. Where space permits and context is appropriate (adjacent to Egypt Park for example), a meandering sidewalk design, similar to a linear park, could be installed to emphasize a more rural aesthetic. Specialty paving could be used in selected areas to mark nodal spaces and to coordinate with other material choices in the overall design guidelines. The pedestrian circulation system should be constructed for universal accessibility where practical.

Paving materials for consideration include: concrete walks, brick (or brick-like concrete pavers, stone pavers, pea gravel or stone/dust walks, and flagstone paving.

f. Bicycle Lane Features

The vision for the Hamlet of Egypt includes bicycle lanes along both sides of Route 31. These lanes should be five feet wide and occur immediately adjacent to the vehicular travel lanes before the curb. The bicycle lane should utilize contrasting pavement color or material and painted markings to better define the designated bicycle lanes. Bicyclists would be allowed access to the bicycle lanes at all curb cuts. At designated locations, bicycle parking should be installed as part of the street furniture system.

g. Trail System

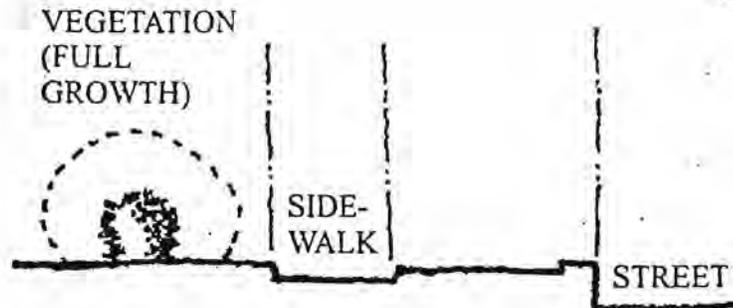
Crossing Route 31 in the project area are two trails, the RS&E Hike/Bike Way and the Crescent Trail. These trails accommodate pedestrians, bicyclists, and other non-motorized means of transit. Clearly marked and signed entrances and connections to the trail system with informational kiosks and other amenities should be installed. Historical interpretive signage referencing the railroad origins of the trails should be installed at a central 'trail head park' envisioned for the intersection of Mason and Loud Roads as both trails would cross Route 31 at this location. The actual crossing could be accomplished with a combined crossing with the local sidewalk system. The landscaping, signage, street furniture, and lighting systems could all link with the trail system at this location.

h. Utilities

Ideally the overhead power lines in the Hamlet would be buried, consolidated behind buildings, or consolidated on one side of the street. However, there are feasibility and cost considerations with these options. Other options for improving the unsightliness of the overhead lines on poles are to consolidate lines on fewer poles, screen with street tree plantings, stain a specific color, use concrete poles, straighten existing poles, and/or utilize poles for other signage to eliminate street 'clutter'.

12. Landscaping

Landscape screens are encouraged along property where vacant lots interrupt the building continuity of the street. Landscape screens shall be set back so that sidewalk encroachment is avoided and street visibility is maintained when the screens grow to full size.



Landscaping Diagram: Full growth of vegetation should not encroach upon sidewalk or obstruct street visibility.

It is highly encouraged that the streets within Egypt be lined with trees. Existing trees should be preserved whenever possible and dead trees within 20' of pavement should be replaced with new trees. A variety of indigenous tree species should be used with broad canopies and 4" minimum caliper trunks. Spacing should be at 30 to 50 feet on center (dependent upon species). Consideration should be given to the horticultural requirements of the individual species, physical limitations of the species, and inherent visual qualities when selecting street trees. Planting locations can be either within the first 10' of the front yard or between the sidewalk and the street (public right-of-way). Colorized mulch (stone, wood, organic, etc.) should be avoided.

In addition to street trees, other plantings in the Subarea should be considered. Other trees, shrubs, and seasonal flower displays should be used to emphasize corridor entrances, complement signage and other street amenities, provide screening, and seasonal color/interest, and to define pedestrian and other non-motorized circulation systems. This additional landscaping could be provided and maintained by municipal, private, civic, or commercial organizations.

13. Economic Development

The 2000 ESR recommended the following in regards to economic development:

- A local Business Association is needed to represent business owners, encourage business growth, and implement property improvements consistent with this plan.
- A mixed used development theme has a greater chance of preserving the economic viability of the Hamlet than continuation of a policy that encourages commercial development of the Route 31 frontage.

It is recommended that a common unifying theme for the commercial center of the Hamlet be developed that is consistent with the character of Egypt. Some potential themes may be Agriculture, Nature (the hills and the trail system), History (the oldest settlement in Perinton) and the Trolley Station. This theme may be integrated both physically, by the aesthetics of the buildings and streetscaping, and by business type (i.e if the main theme is Agriculture, then businesses such as farm markets, country

stores, antique stores, flower shops, dairy/ice cream shops, agriculturally themed restaurants, etc. would be appropriate).

In addition, an economic development survey of local Egypt residents was performed during this committee process has been included in the Appendix. It is recommended that this survey be referenced by those considering commercial development within the Subarea.

HAMLET ARCHITECTURAL GUIDELINES

A. Introduction

The Hamlet Architectural Guidelines are standards that have been created to address specific aesthetic issues within the Subarea and the Hamlet Center. These guidelines are intended to promote appropriately scaled and detailed buildings and shall be considered a refinement to the 2000 ESR Appendix B: Design Guidelines prepared by the Landmark Society.

There are generally two types of architectural guidelines: Prescriptive and Descriptive. Prescriptive guidelines are directed towards a particular style (or styles) of architecture. Descriptive guidelines are directed towards how proposed changes should relate to existing architecture and contextual character. The following guidelines are more descriptive in nature, and are intended to promote new buildings that are compatible with the existing architecture. They are intended to enhance the existing character of the Subarea while also contributing a new layer of history to it. This philosophy is consistent with the Landmark Society study as well as the Secretary of the Interior's Standards for Rehabilitation.

B. Design Guidelines Prepared by the Landmark Society

As a part of the 1999 Comprehensive Plan for the Town of Perinton a study entitled "Identification of Architectural and Visual Elements that Define the Historic Character of the Hamlet of Egypt, Town of Perinton," (1999 LSS) was created by Ann B. Parks and the Landmark Society of Western New York.

This study provides a list of structures that define the historic character of the Hamlet. In order to promote appropriate new buildings, it is recommended that developers refer to the 1999 LSS and the most recent listing of historic buildings within the Subarea to better understand the context of Egypt.

C. Architectural Design Principles

The Design Principles for architectural design shall be consistent with the principles defined in the Hamlet Development Guidelines. In addition, development within the Historic District Boundary, or to a designated historic property, shall be reviewed by the Perinton Historic Architecture Commission.

The Hamlet Architectural Guidelines set forth in this document are intended to support and give form to these Principles. If a question should arise regarding the specific implementation of the Hamlet Development Guidelines or Hamlet Architectural

Guidelines, the Design Principles should be referenced to ensure that decisions are made in keeping with the Subarea vision.

D. Hamlet Architectural Guidelines

I. Scale & Size

The scale and size of new buildings shall be consistent with the existing context of the Hamlet.

a. Height and Stories

<u>Designation</u>	<u>Maximum Height / Stories</u>	<u>Exceptions</u>
Commercial (Hamlet Center)	35' / 2 ½	1
Commercial (Subarea Periphery)	35' / 2 ½	
Business (Hamlet Center)	35' / 2 ½	1
Business (Subarea Periphery)	35' / 2 ½	
Mixed-Use (Hamlet Center)	35' / 2 ½	1
Mixed Use (Subarea Periphery)	35' / 2 ½	
Planned Residential (Hamlet Center)	35' / 2 ½	
Planned Residential (Subarea Periphery)	35' / 2 ½	

Exceptions

1. 40' / 3 is acceptable at the Hamlet Center provided that there are no single-family residential land use areas directly adjacent.

b. Building Footprint

Building footprint requirement shall be per the Town Zoning Code.

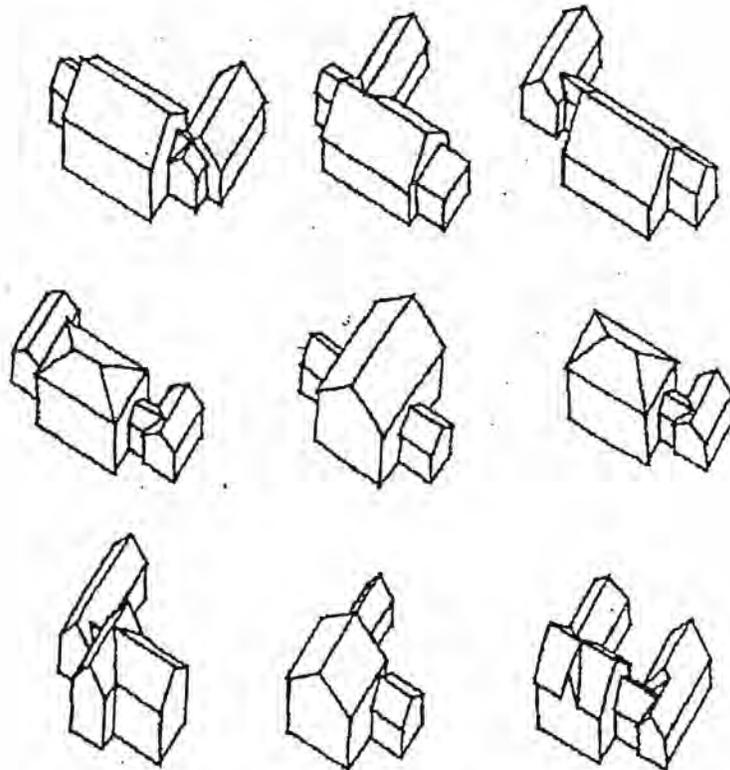
2. Massing & Proportions

a. Massing

Building massing should be simple rectangles or squares with side wings or rear extensions in keeping with the historic precedent of Egypt. In general, smaller buildings should have simpler massing with fewer elements and larger buildings should have more complex massing with more elements.

Residential buildings should be 1 to 2 ½ story volumes primarily with gabled, hipped or shed roof types (unless local historic precedent can be cited, no other roof types are permissible).

Commercial buildings should be 1 to 3 story volumes (contingent upon restrictions) with gabled, hipped or shed type roofs. Parapets are also acceptable on a limited basis upon approval (see Nelson's Store in the 1999 LSS). Commercial buildings, although larger, can have simpler massing provided the bay spacing and proportions of the building comply with the Guidelines.

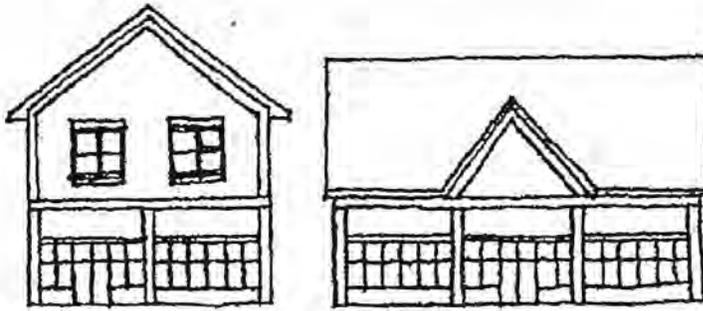


Massing Diagram: There are a variety of appropriate building massing options.

b. Proportions

In general, residential building proportions shall be human scale and consistent with the existing context of the Hamlet.

Commercial buildings shall have consistent bay spacings of 10 to 20 feet. Two and three-story commercial buildings with parapets shall be composed of the following elements: base or water table, storefront, awning or canopy, stringcourse or frieze, and cornice. Precedents of historic Egypt commercial buildings should be referenced for proportions and aesthetics (for example: the Nelson's Store - 7467 Pittsford Palmyra Road, and early structures of the Egypt Canning Company).



Proportions Diagram: Conceptual commercial 'large house' type building proportions.

3. Building Elements

a. Residential Buildings

- **Roofs**

The following roof types are acceptable: gable, hip or shed (sheds only acceptable as secondary roofs). Roof slopes shall be between 5:12 and 12:12 (a slope of 3:12 or greater is acceptable upon approval for ancillary shed roofs). The following roofing material is preferred: architectural asphalt shingles.

- **Facades**

House facades shall be composed of a balanced rhythm of openings (windows and doors) and building elements compatible with regional historic structures (refer to the 1999 LSS). Although literal symmetry is not required, balanced façade compositions are preferred. Bay windows, porticos, porches and other projecting building elements are acceptable provided these elements remain subordinate in proportion to the scale of the façade.

In general front facades should be oriented parallel to the street. There shall be an entrance located on the front façade. The front entrance shall be designed so that it creates a formal public expression to the street. It is recommended that a sidewalk leading to the front entrance be incorporated.

- **Garages and Ancillary Buildings**

Garages and ancillary buildings should be oriented to the rear of the house (see Parking in the Development Guidelines). Garages and ancillary buildings should be subordinate in proportion and detail to the house. Garages and ancillary buildings shall relate architecturally to the house in style, material and color.

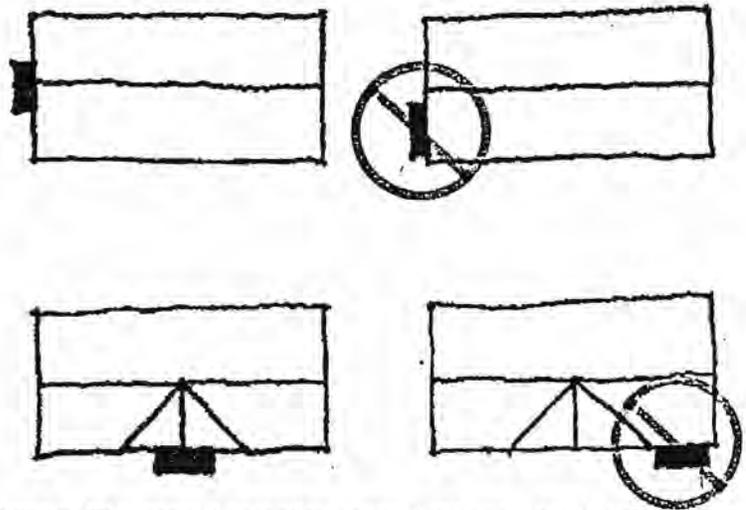
- **Porches**

One-story porches that face the street are encouraged (wrap-around porches are also acceptable). Porches facing the street shall be open (glazing and fixed screens are not acceptable – removable screens are acceptable upon

approval). Porch columns, posts, balusters, etcetera shall be constructed of wood and painted (see Materials). Porch roof materials shall match the primary roof material and color.

- **Chimneys**

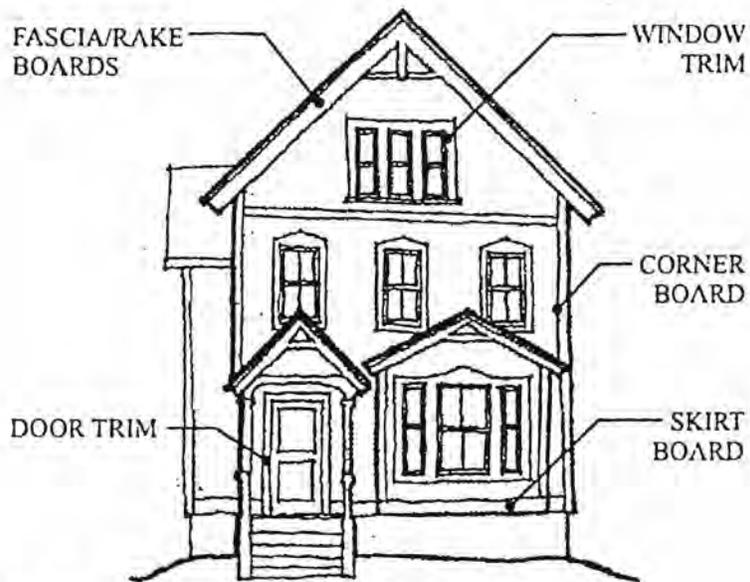
Chimneys shall be constructed of brick or stone. Chimneys shall be proportioned in a manner that is compatible to regional historic buildings. Chimneys should be typically placed centered on the ridge of the gable end (or gable dormer) of the house. All chimneys shall be appropriately capped to conceal spark arresters. All fireplace enclosures and chimneys shall extend down to grade.



Chimney Diagram: Appropriate located chimneys on the left, inappropriate located chimneys on the right.

- **Details**

In general, houses shall be simply detailed in a manner that is compatible to regional historic buildings (refer to the 1999 LSS). The following details shall be incorporated into the house design: corner boards, skirt boards, rake boards, fascia boards, opening trim (see Fenestration), etcetera. The following details may be incorporated into the house design: gable returns, shutters, friezes, cornices, etcetera.



Details Diagram: Appropriate details and trim components.

- **Miscellaneous**

The following shall be permitted provided they are not visible from the public right-of-way: skylights, hvac equipment, solar panels, satellite dishes, antennas, permanent cooking grills, permanent play equipment, hot tubs, swimming pools, etcetera. It is recommended that utility meters be located so that they are not on the front façade.

b. Commercial Buildings

- **Roofs**

The following roof types are acceptable: gable, hip or shed (sheds only acceptable as secondary roofs). Parapets are acceptable on a limited basis upon approval (refer to Nelson's Store for historic precedent). Roof slopes shall be between 5:12 and 12:12 (a slope of 3:12 or greater is acceptable upon approval for ancillary shed roofs). The following roofing materials are acceptable: architectural asphalt shingles and standing seam metal roofs.

Simple roof forms are preferred. Multiple layers of false roof lines (roof lines that have minimal depth, do not relate to the building massing, and are purely decorative) are discouraged.

- **Facades**

Commercial buildings shall be designed so as to be compatible with the local hamlet architecture. Commercial building aesthetics may be of two types: residential-like building or agricultural/commercial building. Residential-like buildings shall follow similar detailing to houses (refer to McGregor's for a local precedent). Agricultural/commercial buildings can take on the following forms: commercial country store, large barn or other similar agricultural structure (refer to Nelson's Store and early Egypt

Canning Company buildings). Another theme from which design could be derived is the historic trolley/train station aesthetic (see 1999 LSS).

The following elements should be integrated into commercial buildings as noted:

Base

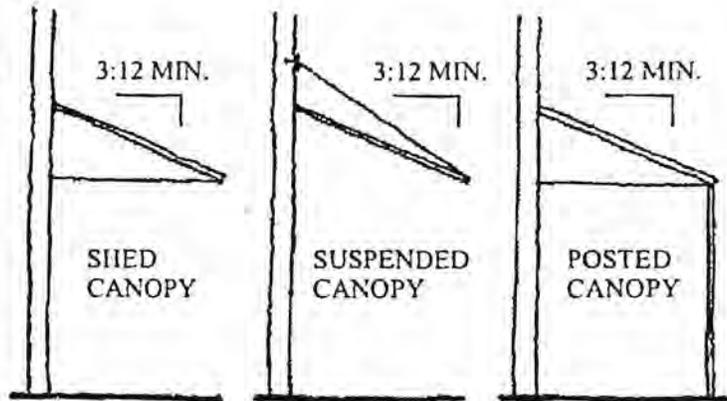
A defined base, generally of 24 to 36 inches in height shall be encouraged. However, in some situations, storefront that extends down to grade may be considered. The base may be constructed of masonry, stone or clad in horizontal siding. The base may also incorporate planter or flower box. (This applies to all Commercial Buildings)

Storefront

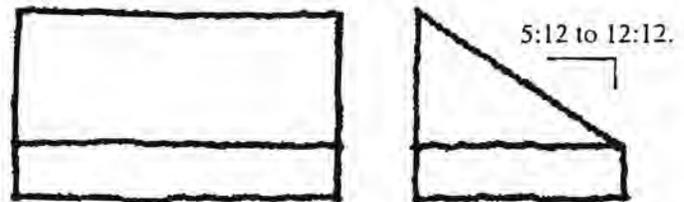
Storefront shall be organized upon a consistent bay spacing (see Proportions). Glazing shall be clear (no spandrel panels are acceptable). Storefront shall extend from the base up to a minimum of eight feet above the finish floor (10' is recommended). Wood, steel and aluminum storefront framing is acceptable and shall be of a color that is compatible with these Guidelines. (This applies to all Commercial Buildings, but not a requirement for Residential-Type Commercial Buildings)

Awning and Canopies

Awnings or canopies are encouraged. Awnings and canopies shall be segmented to fit within bay spacing (not continuous). Awnings and canopies shall be of the following materials: fabric or standing seam metal (other like materials may be considered). Awnings may be fixed or retractable. Fabric awnings shall include a non-scalloped valance minimum 6" in depth. A minimum slope of 3:12 is acceptable for retractable awnings only. Awning corners shall not be radiused, chamfered or faceted. All awnings and canopies shall comply with the proportions of the diagram below. (This applies to all Commercial Buildings, but not a requirement for Residential-Type Commercial Buildings)



Canopy Diagram: Typical canopy types.



Awning Diagram: Typical awning dimensions.

Friezes

Friezes or stringcourses are typically located above the storefront and/or awning. Friezes and stringcourses shall be used to break up the building façade horizontally. Friezes may be used for building signage provided it is in keeping with the Town of Perinton Sign Ordinance. (This applies to all Commercial Buildings, but not a requirement for Residential-Type Commercial Buildings or Barn/Agricultural-Type)

Cornices and Copings

All commercial buildings that have parapets shall have cornices and copings. The cornices should be simply detailed and profiles shall be in keeping with regional historic precedent (see Nelson's Store). All parapets shall be horizontal in elevation as viewed from the street unless articulated in an approved composition. Cornice depths should range from 12" to 36" dependant upon overall proportions of the façade. (This applies to all Commercial Buildings with parapets)

Trim

The following details shall be incorporated into the commercial building design (for large residential-like designs): corner boards, skirt boards, rake boards, fascia boards, opening trim (see Fenestration), etcetera. The following details may be incorporated into the commercial building design (for large residential-like designs): gable returns, shutters, friezes, cornices, etcetera. (This applies to all sided Commercial Buildings, but not a requirement for Barn/Agricultural-Type Commercial Buildings)

- **Garages and Ancillary Buildings**

Garages and ancillary buildings should be oriented to the rear of the building (see Parking in the Development Guidelines). Garages and ancillary buildings should be subordinate in proportion and detail to the main building. Garages and ancillary buildings shall relate architecturally to the building in style, material and color. Dumpsters shall be contained within enclosures so that they are not exposed to view.

- **Details**

In general, commercial buildings shall be simply detailed in a manner that is compatible to regional historic buildings (refer to the 1999 LSS). Agricultural building aesthetics shall be considered.

- **Miscellaneous**

The following shall be permitted provided they are not visible from the public right-of-way: skylights, HVAC equipment, solar panels, satellite dishes, antennas, permanent cooking grills, permanent play equipment, hot tubs, swimming pools, etcetera. Louvers and vents shall not be located on the entrance or street side of the building (if this is unavoidable, these elements shall be arranged in a manner that is compositionally integral with the building façade).

4. Materials and Color

a. Exterior Wall Materials

The primary exterior wall material used within the Hamlet of Egypt Subarea is horizontal clapboard siding. It is highly recommended that all siding and trim be wood clapboard siding or smooth-faced cementitious board. Vinyl siding is discouraged but is acceptable upon approval only if used with a smooth-face finish and aluminum-wrapped corner boards, window and door trims, skirts, fascias, etcetera are provided. Colors shall meet the Architectural Guideline requirements.

Other acceptable exterior wall materials are brick and stone. Decorative concrete masonry units (c.m.u.) are acceptable upon approval and should be used sparingly (refer to local historic precedents of c.m.u. usage).

One-story buildings shall be of one exterior wall material unless otherwise approved (a masonry base or water table is acceptable). Multi-story buildings may be composed of two exterior wall materials provided that the transition between materials is made horizontally at or near the second floor elevation.

b. Color

Building colors within the Historic District Boundary shall comply with the Color Chart below. Building colors outside of the Historic District Boundary, but within the Subarea Boundary, shall be compatible with those of the Hamlet Center. Walls shall not be more than one color per material. Trim shall not be

more than one color (two is acceptable upon approval). No more than one accent color (for example: front door or shutters) is acceptable. Brick (c.m.u.) and Stone colors that are compatible with historic regional structures are acceptable. Brick with a contrasting range is not acceptable.

The following color chart is based upon the Sherwin-Williams historic palette. It has been provided as a color reference only. Other manufactures are acceptable.

Color Chart

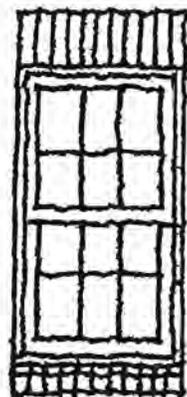
<u>Area</u>	<u>SW Paint Color</u>
<i>Historic District</i>	sw2801 through sw2832
Subarea (outside <i>Historic District</i>)	colors compatible with Hamlet Center
<u>Area</u>	<u>Brick (CMU) and Stone Color</u>
<i>Historic District & Subarea</i>	compatible with regional historic structures

5. Fenestration

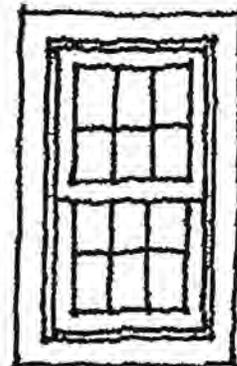
a. Windows

Windows shall be constructed of painted wood, solid vinyl, vinyl-clad or painted aluminum-clad (vinyl and aluminum windows shall be subject to approval). All glazing shall be clear glass. Windows shall typically be single or double-hung. Divided lites are preferred but not required. The following types of divided lites are acceptable: true divided lites, simulated true divided lites, and exterior/interior muntin grids. Divided lites with no exterior profile are not acceptable. All muntins shall have a profile on the exterior of the window that provides a shadow. Trim around framed openings shall be 3 1/2" wide minimum. Masonry openings shall include brick soldier, pre-cast concrete or stone heads, and brick rowlock, pre-cast concrete or stone sills. See the diagram below for proportion requirements.

When shutter are used their height shall match the height of the window opening and their width shall be equal to 1/2 the width of the window opening.



MASONRY OPENING



FRAMED OPENING

Window Diagram: Typical window proportions should be approximately 2:1 minimum (height to width).

Simple rectangular window designs are preferred. Ornate windows that are uncharacteristic of the Hamlet, such as paladian windows, are discouraged.

b. Doors

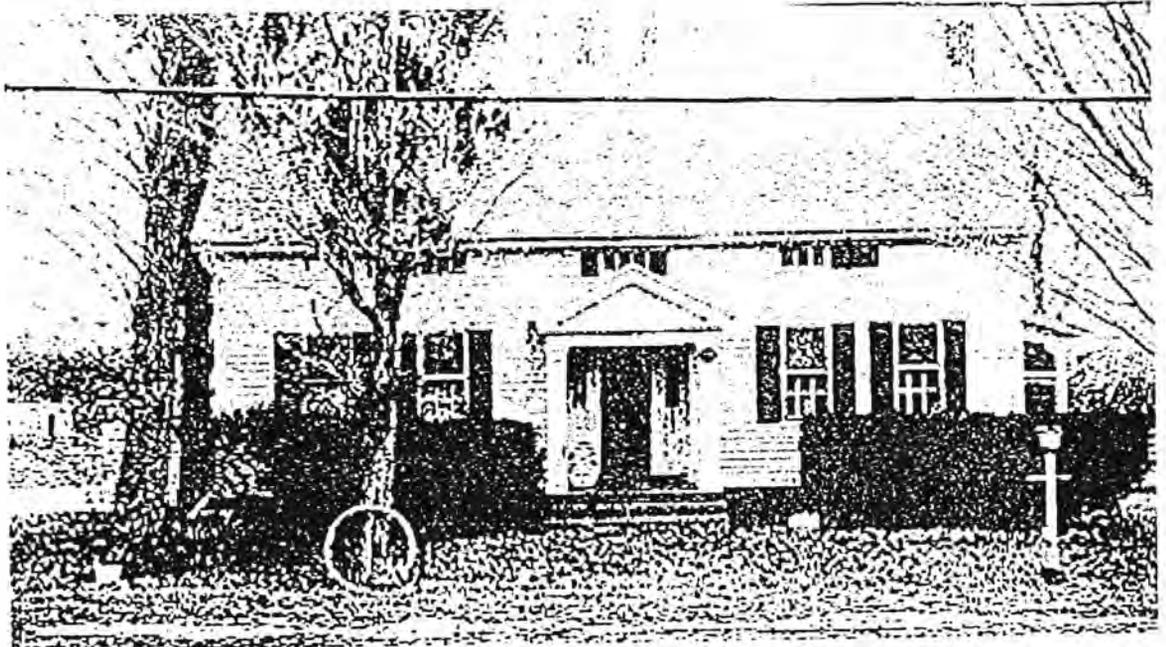
Doors shall be painted (or stained upon approval). Principal entry doors shall face the street (front yard). Doors located within storefront may be of the storefront material (commercial buildings only).

c. Storefronts

See Commercial Buildings Facades.

6. Existing Buildings

The following images are some examples of Hamlet structures that contribute to the character of Egypt. For a more comprehensive documentation of the historic architecture of Egypt refer to the 1999 LSS:



7516 Pittsford -Palmyra Road



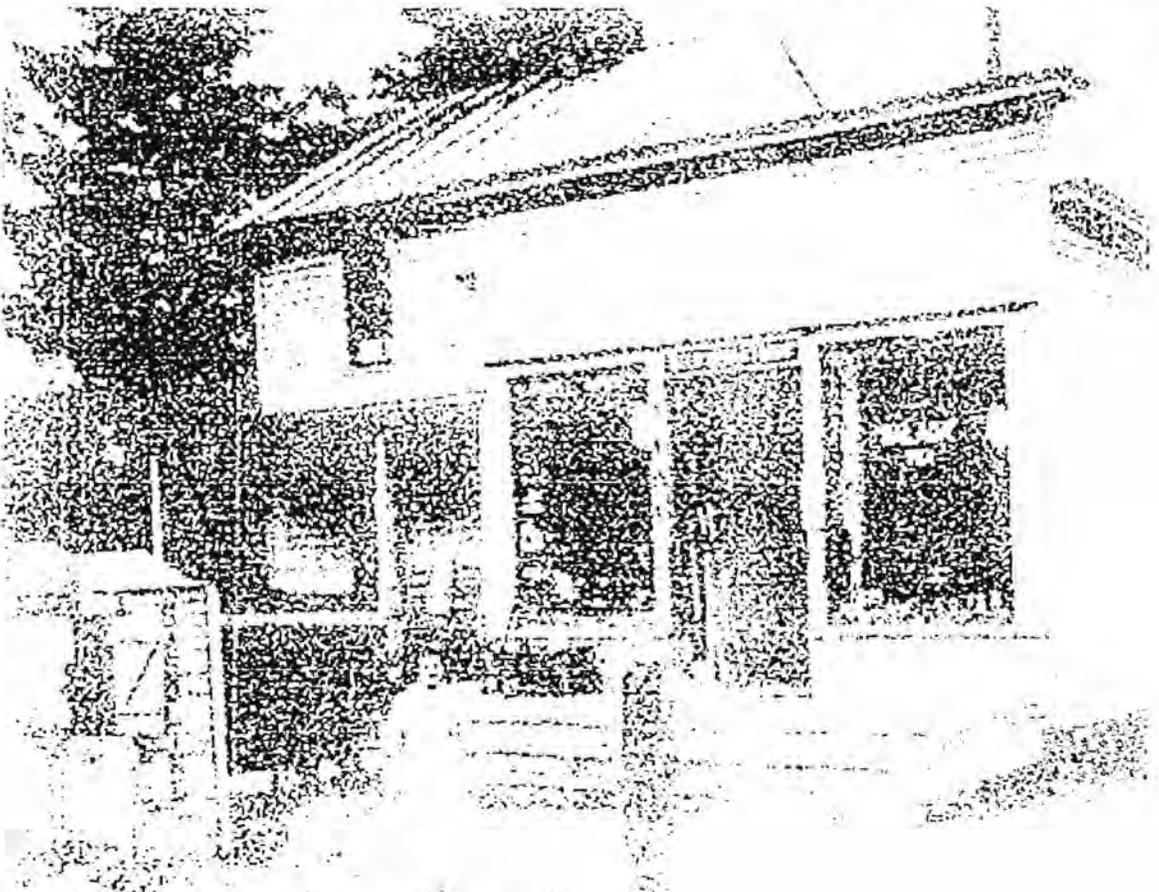
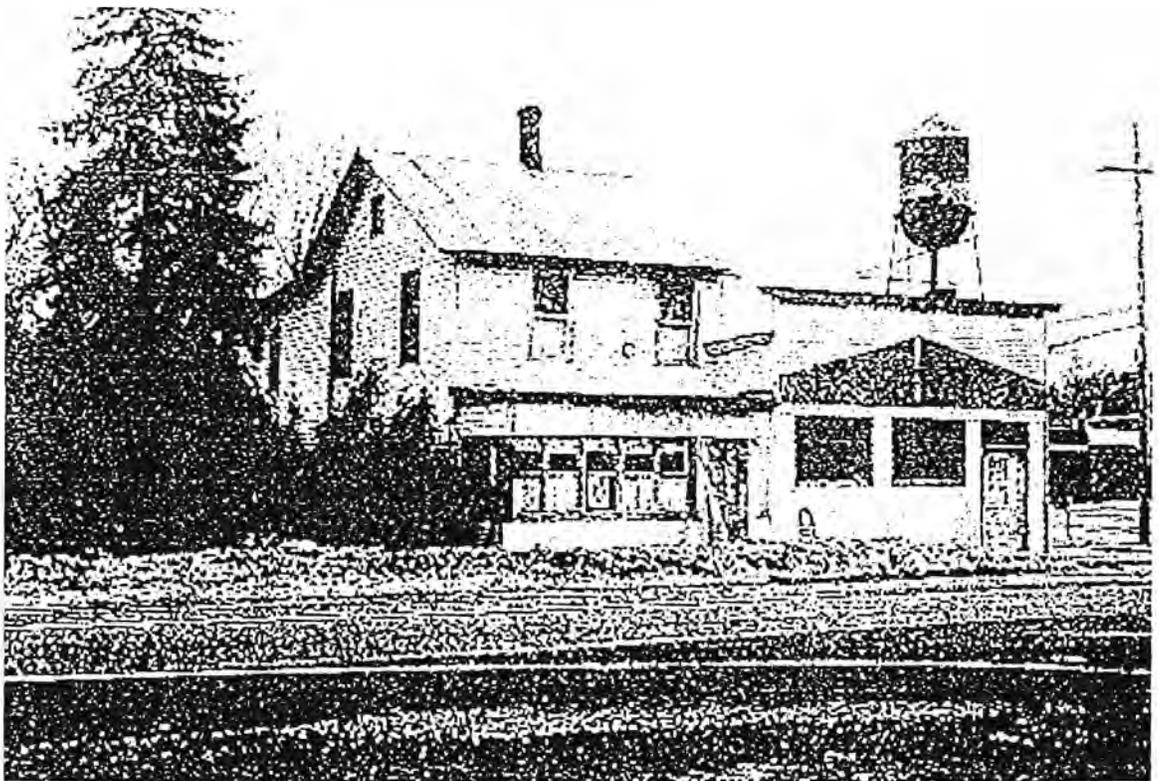
7420 Pittsford -Palmyra Road



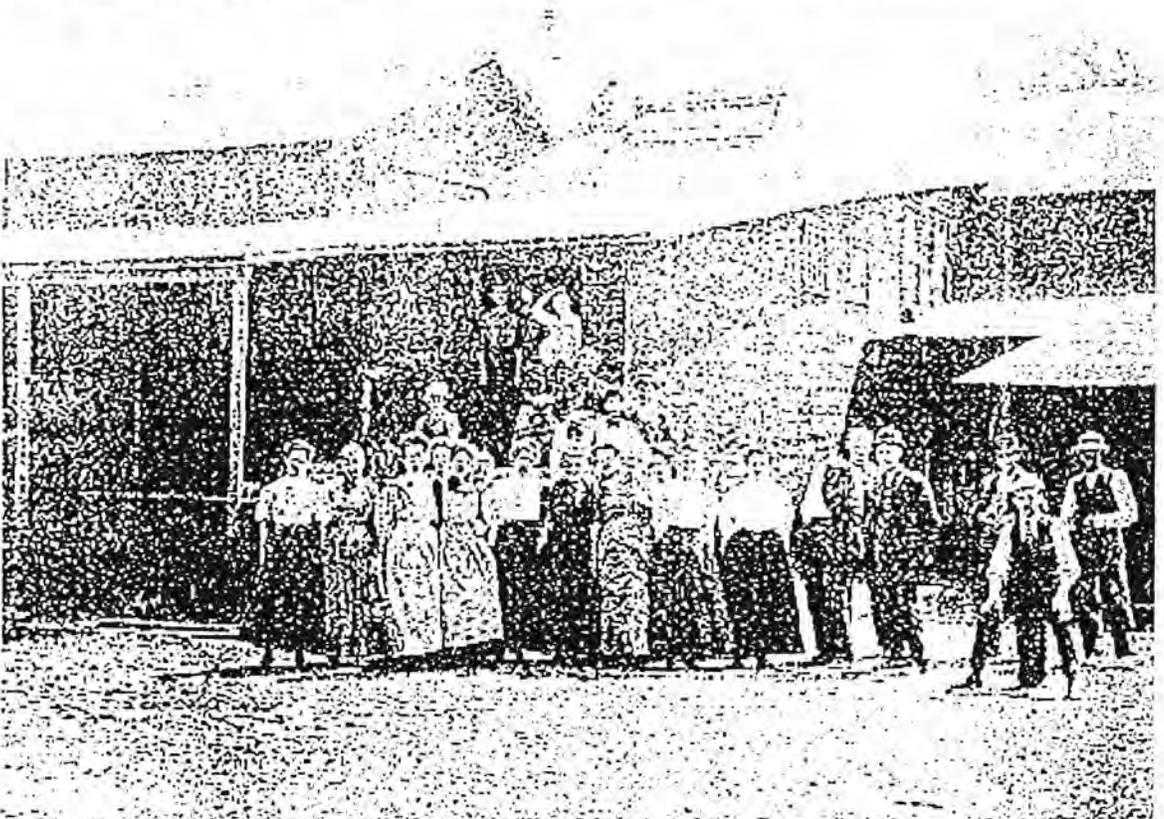
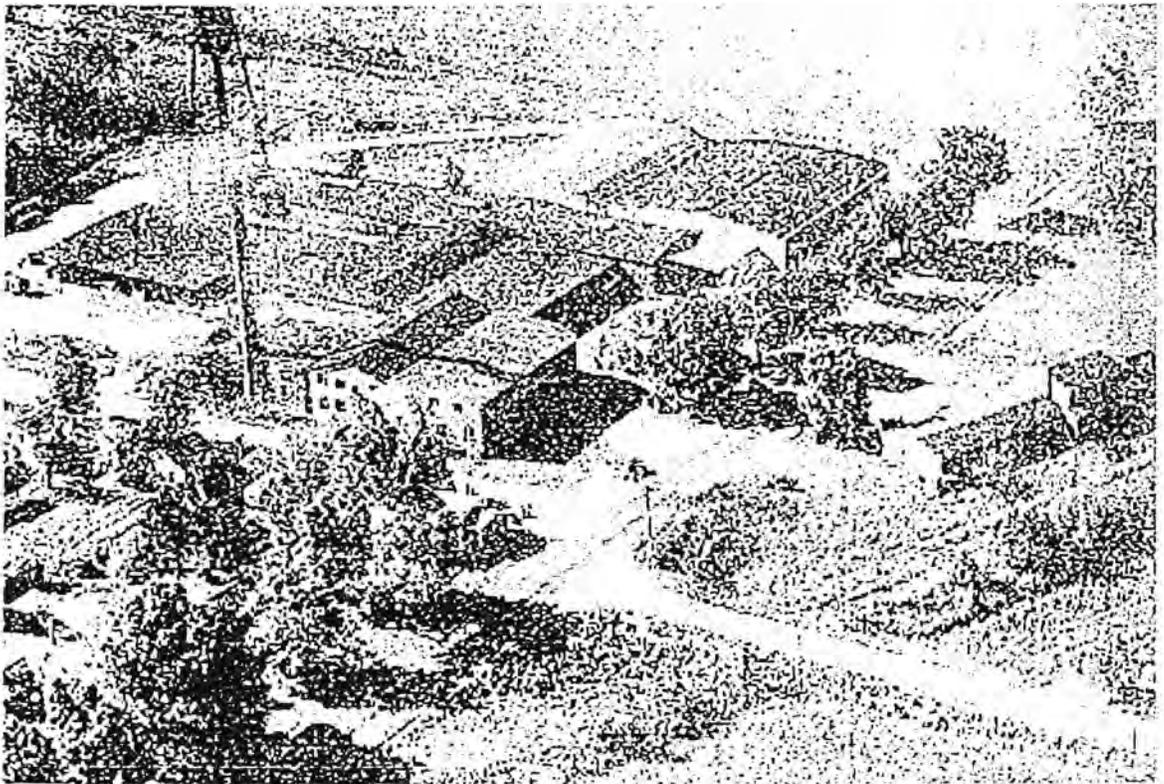
353 Mason Road



7559 Pittsford - Palmyra Road



7467 Pittsford - Palmyra Road (Nelson's Store)



Egypt Canning Company (Comstock Canning Company)



Oliver Loud's Tavern (Relocated to Bushnell's Basin)



Egypt Trolley Shelter

GLOSSARY OF TERMS

Access Management:	The application of state and local planning, capital improvement, regulatory and management tools to enhance and/or preserve the transportation function of the roadway by reducing the number of access points.
Awning:	A roof-like structure, as over a door, window, or storefront, that serves as shelter from the weather. Awnings are constructed of various materials typically fabric or metal. Awnings can be fixed or retractable. Unlike canopies, awnings are more temporary building elements that can be removed.
Base / Water Table:	A horizontal band in a wall, typically constructed of stone or masonry, that meets the ground plane. Usually a base or water table is integral with the foundation wall of a buildings and extends up to a consistent height.
Buffer:	A land area or landscaping treatment which is used to visibly separate one use from another or to shield or block noise, lights or other nuisances.
Corner Board:	Trim used at the corner of buildings to provided a termination for siding.
Cornice / Coping:	A cornice is any molded projection, often decorative, that crowns or finished the part of which it is affixed (typically the top of a wall). A coping is a cap to a parapet wall and may act integrally with or as a cornice. Copings are typically constructed of stone or concrete when integral with wall, and metal when covering another material.
Canopy:	A canopy is a roof-like structure, typically over a door or porch, that serves as shelter from the weather. Canopies are usually integrated into the design of a building and are more permanent than awnings. Canopies are typically constructed from like-materials of the building.
Density of Development:	The number of dwelling units per unit of land. Unit of land is usually stated per acre, (i.e., 2 units per acre, 4 units per acre, one unit per 5 acres, etc.).
Density Gradation Diagram:	The term Density Gradation Diagram, as used in this document, refers to a graphic representation of changing density levels progressing from the center of the study area outward. It is used as a general model for consideration in planning the land use. Density is considered as both units per acre and size and mass of building.
Fascia / Rake Board:	Trim that terminates the edge of a roof (the gable end is typically termed rake board) and transitions between roof and soffit.
Frieze (Frieze Board):	Horizontal band located typically just the below the cornice or just above column capitals (incorporated with a stringcourse). Frieze boards located above the storefront of a commercial building can be used as a sign band.
Limited Development District (LDD):	Areas in the Town that have been identified as having special environmental conditions which limit the area's suitability for development. Special environmental conditions include slopes equal or exceeding 15% grade, areas with moderate or high susceptibility to flooding and ponding, and stream corridors. Residential units cannot be developed on lots less than five acres in LDD areas without review by the Conservation and Planning Board or by the Zoning Board of Appeals.
Mixed Use Area (MXA):	These are areas where commercial, office and residential uses could intermix, either in separate parts of the development, separate buildings, or mixed uses in the same building. These could be new developments or redeveloped properties. They would be planned with a unifying theme or style appropriate for the specific area being

developed or redeveloped, and could include amenities for tenants and visitors/shoppers. Mixed use areas could have both planned and single use projects. There currently is not a corresponding zoning category, but similar development patterns have occurred in several areas of Town.

Planned Residential (PR):

These are areas planned as whole, often with combinations of housing styles and densities and/or the incorporation of recreational amenities. Parts of the development could be zoned differently to accommodate the type of development within it, or the project could be approved using Section 278 of Town Law.

Planned Residential Business/Amenity (PRB):

A Sub-Designation of Planned Residential for areas that primarily serve as transitions from Planned Residential areas to areas of greater density and/or business, commercial or mixed use areas. These areas are intended for resident oriented amenities and restricted services such as parks, clubhouses, recreation facilities, etc. The scale, proportion, and aesthetics shall be compatible with both the adjacent residential and commercial areas. These areas are intended to be linked to the residential areas and local trail systems by a comprehensive pedestrian circulation plan.

Planned Residential Low-Density (PRL):

A Sub-Designation of Planned Residential for areas that primarily serve as transitions from Suburban Residential to areas of greater density. Housing densities of 3-6 units per acre on lot sizes ranging typically from 7,000 to 11,000 square feet. Single-family houses are strongly encouraged. Duplex units are not encouraged, but if proposed they must not be predominant, must meet the Hamlet Architectural Guidelines, must be substantially buffered to an existing adjacent single-family land use areas, and must express variety in their architectural treatment. These areas are intended to be oriented in a village-like configuration and linked to the neighborhood and local amenities by a comprehensive pedestrian circulation system. The house footprints are intended to be oriented towards the front yard so that the front facade contributes to the pedestrian character of the street. Rear yard vehicular access is encouraged, but not required.

Planned Residential Medium-Density (PRM):

A Sub-Designation of Planned Residential for areas that primarily serve as transitions from Planned Residential Low Density to areas of greater density and/or business, commercial or mixed use areas. Housing densities of 6 to 8 units , single or multi-family, per acre. These areas are intended to be oriented in a village-like configuration and linked to the neighborhood and local amenities by a comprehensive pedestrian circulation plan. Parking ratios are intended to be limited to 1-2 spaces per unit. Rear yard vehicle access is encouraged, but not required. Townhouses and Condominiums are encouraged, apartments are acceptable.

Right-of-Way:

A strip of land which is acquired, usually by a government body, for the purpose of a road, crosswalk, railroad, utility lines, or other similar uses.

Setback:

The distances between the street right-of-way and the front line of a building or any projection thereof, or the minimum prescribed distance to a lot line that a structure can be placed by town regulation.

Skirt Board:

Horizontal trim that terminates the bottom of exterior siding and transitions to the base or foundation wall below.

Shared Access Parking:

A provision for two or more separate uses to use the same parking area.

Storefront:

Storefront refers to a system of large bayed metal or wood framed glass typically used for commercial buildings.

Stringcourse:

A horizontal band of trim that is used as ornament or to transition from one material to

another (can be wood or masonry).

Town Law Section 278: Section 278 of the New York State Town Law permits towns to reduce lot sizes, building setbacks and other dimensional features in subdivisions if the overall density of the subdivision is no greater than it would have been using conventional development techniques. Commonly used as a legal basis for creative design to preserve open space.

**Transitional Land Use
(Transition Zone):**

- 1) Type of use that serves as a buffer between two other types of uses, (e.g., commercial uses between industrial and residential uses).
- 2) A type of use that is allowed for a period of time until an action occurs that allows a change in that use, (e.g., five-acre minimum lots until sewer or water service is made available, at which time smaller lots are allowable).

REFERENCES

(document abbreviation in italics)

"Town of Perinton Comprehensive Plan – Update 2000." *(2000 CP)*

"Town of Perinton Comprehensive Plan Update - 2000: Egypt Subarea Report and Recommendations." *(2000 ESR)*

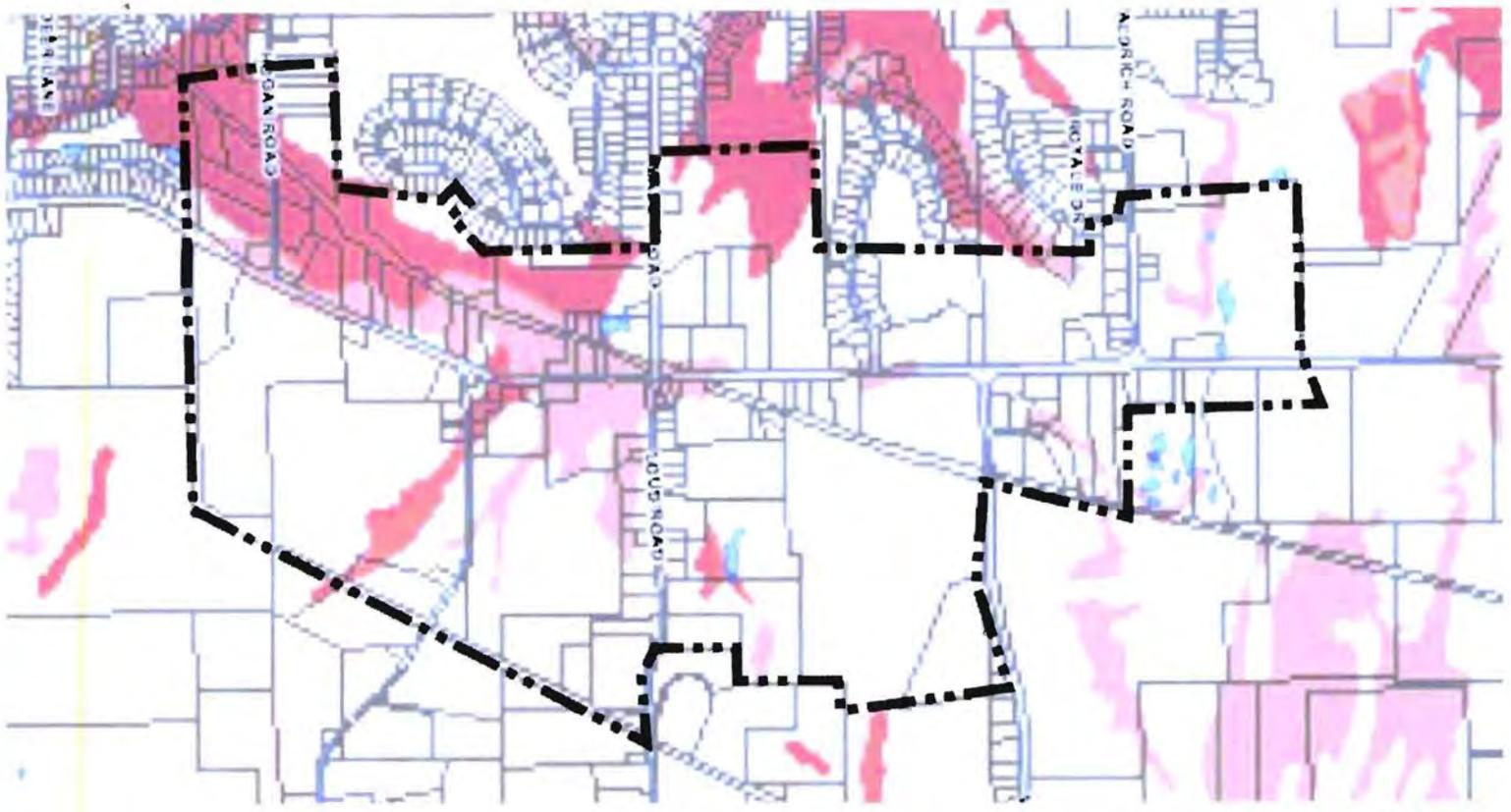
"Town of Perinton Comprehensive Plan Update – 2000: Guidelines for Environmental Protection and Growth Management – A Supplement to the Comprehensive Plan." *(2000 EPG)*

"Identification of Architectural and Visual Elements that Define the Historic Character of the Hamlet of Egypt, Town of Perinton." The Landmark Society of Western New York, March 1999. *(1999 LSS)*

"NYS Route 31/Hamlet of Egypt Transportation Study 2001, Town of Perinton." *(2001 ETS)*

"Hamlet Design Guidelines." The Dutchess County Department of Planning and Development: New York Planning Federation, October 1994.

"Building Form Guidelines." The Dutchess County Department of Planning and Development: New York Planning Federation, October 1994.



LEGEND

SLOPE IS 15% OR GREATER

MODERATE SUSCEPTIBILITY
TO FLOODING & PONDING

HIGH SUSCEPTIBILITY TO FLOODING & PONDING

WATER FEATURES

PARCEL BOUNDARIES

SCHOOL DISTRICTS

VILLAGE OF FAIRPORT

TOWN OF PERINTON

EGYPT SUBAREA BOUNDARY

LIMITED DEVELOPMENT DISTRICT (LDD) MAP

KEY ISSUE ASSESSMENT APPROACH

HAMLET IDENTITY		
1 Sense of Community	Develop New and/or Enhance Existing Local Services, Public Amenities, etc. Create Pedestrian Circulation Plan Unifying Architectural Theme Through Land Use Plan (& Zoning) Through Land Use Plan (& Zoning) Mitigate the Impact of 31 through Traffic Calming, Landscaping, Hardscaping Integrate the Hamlets History with Transportation with 31 View the 4-Corners as the Hamlet Center Identify Local (and Through-Traffic) Services/Retail Needs Easy Access on/off 31 Architectural Theme (Historical/Agricultural) Destination Point - Pedestrian Oriented Historical/Agricultural/Recreation/Natural Also See Items 1 & 5 Historic Site Specific Details Gateways, Defined Borders, Density, Architectural Theme (Historical/Agricultural) See Items 1,2,3,5,6 Historical Influence, Human/Hamlet Scale See Items 1,2,6,7,8 See Items 4,8 Local Landmarks, Focal Points (Water Tower) Geographic Features, Gradual Building Density Increase See Items 4,6,8,10 Identify Existing Vistas and Geographic Features of the Hamlet See Items 6,11 Research Precedents	Resident Survey Results (Next Phase) Density Gradation Diagram Traffic Circulation Plan (Next Phase) Design Guidelines (Next Phase) Proposed Land Use Diagram Proposed Land Use Diagram Traffic Safety Measures (31 DOT Work) Design Guidelines (Next Phase) Density Gradation Plan Traffic Capacity Analysis (31 DOT Work) Resident Survey Results (Next Phase) Traffic Capacity Analysis (31 DOT Work) Design Guidelines (Next Phase) Traffic Circulation Plan (Next Phase) Design Guidelines (Next Phase) Design Guidelines (Next Phase) Transportation Study (31 DOT Work) Design Guidelines (Next Phase) Design Guidelines (Next Phase) Design Guidelines (Next Phase) Density Gradation Plan (Land Use Plan) Design Guidelines (Next Phase)
2 Grouping Buildings Open Space & Parks		
3 Core Developed Periphery Open		
4 Route 31 Divides Hamlet		
5 Attractive Place to Shop		
6 Hamlet should be a Destination Point		
7 Unique Features Incorporated into New Elements		
8 Sense of Place should be Established		
9 Design Theme Should be Established		
10 Gateway and Corridor Treatments are Important		
11 Sense of Arrival to the Hamlet (Boundary)		
12 Views and Vistas Maintained and/or Enhanced		
13 Local and National Precedents should be Examined		
TRAFFIC PATTERNS/CIRCULATION		
14 Traffic On and Off 31 (Primarily Residents, Safety)	Examine Transportation Study: Traffic Calming, Turning Lanes, Limit Curb Cuts, Secondary Access Roads, etc. Examine Transportation Study Examine Transportation Study Examine Transportation Study Examine Transportation Study See Items 5,6,8,11 Architectural Guidelines See Items 5,7,9 Examine Transportation Study Through Land Use Plan (& Zoning) Master Planning, See Items 1,14	Apply Lessons to Design Guidelines Transportation Study (31 DOT Work) Traffic Capacity Analysis (31 DOT Work) Traffic Circulation Plan (Next Phase) Traffic Capacity Analysis (31 DOT Work) Traffic Circulation Plan (Next Phase) Traffic Capacity Analysis (31 DOT Work) Transportation Study (31 DOT Work) Design Guidelines (Next Phase) Design Guidelines (Next Phase) Traffic Circulation Plan (Next Phase) Proposed Land Use Diagram
15 Traffic Through Hamlet (East & West, Towards Mall)		
16 Protect Loud & Thayer from High Traffic (Mall)		
17 Trip Generation Calculations to be Revisited		
18 Motorist Encouraged to Stop at Hamlet vs. Through		
19 Concern of Negative Architectural Impact from 31		
20 Internal External Circulation (Vehicle & Pedestrian)		
HISTORIC CHARACTER / HISTORIC PRESERVATION / CREATION		
21 Small Village Character is Important	See Items 1,2,3,6,7,8,9,10,11,13,16 See Items 1,2,20 Architectural Guidelines, Proportions, Massing, Details, Pedestrian-Oriented, etc. Appropriate Adjacent Land Use Architectural Guidelines See Items 7,9 Architectural Guidelines See Items 7,9 Through Land Use Plan (& Zoning) Development Incentives Master Planning See Items 1,2,8,22,23	Design Guidelines (Next Phase) Traffic Circulation Plan (Next Phase) Design Guidelines (Next Phase) Proposed Land Use Diagram Design Guidelines (Next Phase) Design Guidelines (Next Phase) Proposed Land Use Diagram Town Development Guidelines
22 Pedestrian Oriented / Friendly		
23 Human Scale		
24 Historic Cemetery should be Preserved		
25 New Developments to Promote Uniqueness/Quality		
26 Incorporation of Theme Expressed in Arch./Landscape		
27 Semi-Public Spaces Incorporated in New Developments		

This Key Issue Assessment Chart is intended to document and track the Key Issues surrounding the Egypt Subarea as defined by the Hamlet of Egypt Master Plan Advisory Committee:

- 1) The first column documents the individual issue by number.
- 2) The second column defines the Key Issue.
- 3) The third column indicates the strategy (strategies) to assess and/or address the Key Issue.
- 4) And the fourth column indicates a potential method of approach for the Egypt Master Plan refinement.

ECONOMIC VITALITY OF THE HAMLET

- 28 Balance of High / Low Density
- 29 Re-Build Sense of Community w/ Comm. Development
- 30 Existing (or Additional) Density may not Support C.D.
- 31 New Buildings have been Successful (Monro Muffler)
- 32 Types of Businesses/Services Desirable to Residents
- 33 Market Studies of Econ. Dev. Vs. Population Density
- 34 Services with Walking Distance should be Considered

Through Land Use Plan (& Zoning)
Master Planning, Buffering,
Adjacencies
Promote Appropriate Needed Services
See Items 1,2,3,5,6,21,22,23,27
Review Economic Development
Timeline, Review Precedents
Review Projected Trip Gen. on 31
Architecture can Increase Business
Town Involvement in What and How
Commercial Development Occurs
Homeowners Survey
See Items 1,5,29
Review Economic Development Data and
Timeline, Review Precedents
See Items 30,31
See Items 1,5,20,22,29

Proposed Land Use Diagram

Density Gradation Diagram
Resident Survey Results (Next Phase)
(Next Phase)
Econ. Dev. Study (Beyond Scope - Future?)
Precedent Analysis
Traffic Capacity Analysis
Design Guidelines (Next Phase)

Econ. Dev. Study (Beyond Scope - Future?)
Resident Survey Results (Next Phase)
(Next Phase)
Econ. Dev. Study (Beyond Scope - Future?)
Resident Survey Results (Next Phase)

Traffic Circulation Plan (Next Phase)

- BUFFERING / LAND USE / ADJACENCIES**
- 35 Sensible Land Use Adjacencies should be Considered
 - 36 Visual Buffering / Topography
 - 37 Light and Noise Buffering
 - 38 Challenge of Transition from Industrial to Residential
 - 39 Continuity vs. Abrupt Land Use Transitions
 - 40 Mixed Use Rezoning should be Considered

Through Land Use Plan (& Zoning)
Buffering Requirements (See 12,28,34)
Preservation of Existing Natural Buffers
Creation of New Buffers for Differing
Adjacent Land Uses (See 12,16,28,35)
Preservation of Existing Natural Buffers
Creation of New Buffers for Differing
Adjacent Land Uses (See 12,15,16,17,35)
Through Land Use Plan (& Zoning)
Buffering / Transitional Land Use
See Items 12,15,16,17,28,35
Gradual Density Transitions
Through Land Use Plan (& Zoning)
Architectural Guidelines (See 3,11,38)
Through Land Use Plan (& Zoning)
Examine Precedents
See Items 3,5,6,8,13,21,22,27,28,29,32,34

Proposed Land Use Diagram
Design Guidelines (Next Phase)
Proposed Land Use Diagram
Design Guidelines (Next Phase)

Proposed Land Use Diagram
Design Guidelines (Next Phase)

Proposed Land Use Diagram
Design Guidelines (Next Phase)

Density Gradation Diagram
Proposed Land Use Diagram
Design Guidelines (Next Phase)
Proposed Land Use Diagram
Apply Lessons to Design Guidelines

DENSITY OF EXISTING AND NEW DEVELOPMENTS

- 41 Concern of Monotony and Repetition of New Develop.
- 42 Concern about Vast Apartment Parking Lots (& Cars)
- 43 Concern of Pride & Maintenance of Renters vs. Owners
- 44 Ownership Desired over Transient Population
- 45 Renter vs. Ownership Issues: General Perception
Increase in Crime
Decreased Property Value
Large Scale Buildings are Out of Context
- 46 Developer is the Owner of Apartments
- 47 Target Tenants for Apartments
- 48 Plan for this Area must be Buildable (Econ. & Tech.)
- 49 Flexibility of Developments to meet Market Trends
- 50 Tax Implications of Rental vs. Owned Properties

Architectural Guidelines
See Items 7,9,21,25,26
Through Land Use Plan (& Zoning)
Architectural Guidelines, Buffering
Parking/Unit Limitations
Hold Developers Accountable for Quality
Designed and Maintained Developments
See Items 1,8,25
Beyond Scope of Design
Beyond Scope of Design
Research Precedents / Data
Research Precedents / Data
Architectural Guidelines
Hold Developers Accountable for Quality
Designed and Maintained Developments
Beyond Scope of Design
Developer Provided Market Research
Market Input from Developers
Through Land Use Plan (& Zoning)
Quality Design that is Appropriate and has
Longevity Regardless of Use Over Time
Data from Town Assessor

Design Guidelines (Next Phase)

Design Guidelines (Next Phase)
Proposed Land Use Diagram

Design Guidelines (Next Phase)

Design Guidelines (Development Quality)
Design Guidelines (Development Quality)
Design Guidelines (Next Phase)
Design Guidelines (Next Phase)
Proposed Land Use Diagram

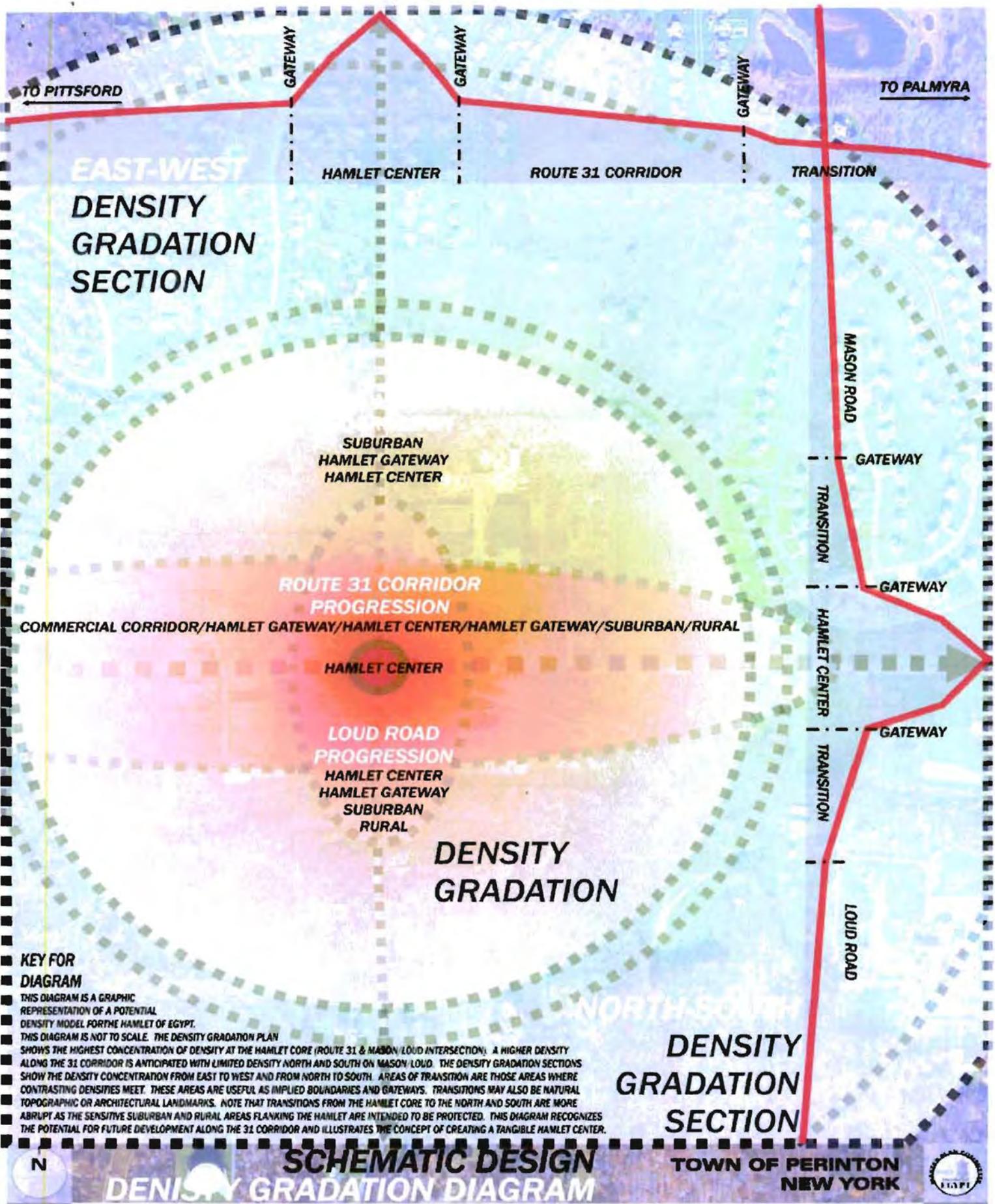
Developer Provided Information
Developer Provided Information
Proposed Land Use Diagram
Design Guidelines (Next Phase)
Developer Provided Information
(Future Analysis)

WETLANDS / FLOODING / EXISTING ECOLOGY

- 51 Concern that the Wetland be Preserved
- 52 Concern about other Environmental Impacts
- 53 Concern about Infrastructural Concerns

Through Land Use Plan (& Zoning)
Through SEQR Process
See Items 12,36
See Item 51
Data Provided by Town
Hold Developers Accountable to Upgrade
Infrastructure as Required for New Constr.

Proposed Land Use Diagram
EIS/SEQR Process (NYSRHPHO)
Limited Development District Map
EIS/SEQR Process (NYSRHPHO)
Town Development Guidelines



KEY FOR DIAGRAM

THIS DIAGRAM IS A GRAPHIC REPRESENTATION OF A POTENTIAL DENSITY MODEL FOR THE HAMLET OF EGYPT. THIS DIAGRAM IS NOT TO SCALE. THE DENSITY GRADATION PLAN SHOWS THE HIGHEST CONCENTRATION OF DENSITY AT THE HAMLET CORE (ROUTE 31 & MASON/LOUD INTERSECTION). A HIGHER DENSITY ALONG THE 31 CORRIDOR IS ANTICIPATED WITH LIMITED DENSITY NORTH AND SOUTH ON MASON/LOUD. THE DENSITY GRADATION SECTIONS SHOW THE DENSITY CONCENTRATION FROM EAST TO WEST AND FROM NORTH TO SOUTH. AREAS OF TRANSITION ARE THOSE AREAS WHERE CONTRASTING DENSITIES MEET. THESE AREAS ARE USEFUL AS IMPLIED BOUNDARIES AND GATEWAYS. TRANSITIONS MAY ALSO BE NATURAL TOPOGRAPHIC OR ARCHITECTURAL LANDMARKS. NOTE THAT TRANSITIONS FROM THE HAMLET CORE TO THE NORTH AND SOUTH ARE MORE ABRUPT AS THE SENSITIVE SUBURBAN AND RURAL AREAS FLANKING THE HAMLET ARE INTENDED TO BE PROTECTED. THIS DIAGRAM RECOGNIZES THE POTENTIAL FOR FUTURE DEVELOPMENT ALONG THE 31 CORRIDOR AND ILLUSTRATES THE CONCEPT OF CREATING A TANGIBLE HAMLET CENTER.

HAMLET OF EGYPT

Hamlet of Egypt Economic Development Survey

Total No. of Surveys: 58

1) Why do you choose to live in the Egypt Subarea? (check all that apply)

		% of Responses	Additional Comments:
schools	32	55%	originally
taxes	16	28%	
electric rates	40	69%	good rates
community / quality of life	50	86%	
convenience	27	47%	shopping areas
price, quality or state of housing	36	62%	competitive
proximity to family or	15	26%	
proximity to work	10	17%	retired now but was very important when I worked

any other reason

When we first bought our home almost 30 years ago, we liked the tranquility of Mason Road.

We moved here because we liked the type of housing and growing single family neighborhoods.

Quiet rural atmosphere

I was born & raised here and choose to stay

Lack of traffic/5+ acres/house

(no other items checked) - Because it was a rural area near shopping.

Rural, removed feel to location.

Open spaces.

Quiet, bedroom community for families.

Shopping convenience, Eastview Mall

Proximity to Rochester, yet still farmland.

Open space -> quantity of.

Quiet natural setting

(none selected) Mason Rd. bordered on the country in 1970 when we moved here. We had country in back of us (now Bent Oak) and country in front of us (proposed to be transformed by PrideMark).

Historical homes.

Beauty of the area.

Availability of private land for purchase.

The neighborhood - out of the way yet close to everything.

Parks & recreation.

Quiet area, privacy, abundant wildlife.

Peace & quiet, large properties, tranquility.

Green areas.

Peace and serenity.

Love the views and the sense of being quieter than other parts of Perinton.

Hamlet of Egypt Economic Development Survey

2) How often do you shop in the Hamlet of Egypt during the week?

		% of Responses
5 or more days a week	0	0%
3 to 4 days a week	2	3%
1 to 2 days a week	8	14%
dally	0	0%
weekly	8	14%
monthly	6	10%
rarely	28	48%
never	5	9%
	57	

Additional Comments noted:

• *takeout pizza*

* *only for Perinton Pizza*

Do my shopping at Eastview Mall or Tops

3) How often do you shop in the Village of Fairport during the week?

		% of Responses
5 or more days a week	0	0%
3 to 4 days a week	1	2%
1 to 2 days a week	11	19%
dally	0	0%
weekly	5	9%
monthly	12	21%
rarely	23	40%
never	4	7%
	56	

Additional Comments

• *(rarely) - too expensive & few selections*

* *But often use the Fairport Public Library.*

Hamlet of Egypt Economic Development Survey

4) For the following Items please Indicate (bold the percentage) the closest percentage of time that you purchase the item in the Hamlet of Egypt. For those items not provided in Egypt place an "X" in the last column for those items that would like to see in the Hamlet of Egypt.

	Survey Version #1				Would it be available in Egypt	Survey Version #2	
	Never	Sometimes	Weekly	Daily		0-50% - Rarely / Sometimes	50-100% - Sometimes / Most Often
Groceries	20		1		9	13	1
Gen. Merch.	19	3			9	9	
Restaurants	3	25	2		6	12	4
Take Out Food	3	22	6		1	11	5
Taverns, Bars	16	10	3			10	1
Flowers	14	10			2	9	3
Crafts, Antiques	22	1			12	8	
Clothing, Fabric	20	1			8	8	
Dry Cleaning	16	7	2		3	8	1
Tailoring	12	10			6	9	1
Shoe Repair	19	1			11	8	
Banking	21	2	1		8	7	1
Electronics	19	1			8	7	
Toys	21				4	7	
Gas	4	18	11		1	9	10
Auto Repair	10	17				10	3
Sporting Goods	18				5	8	
Hair Styling	16	4			2	9	3
Legal Service	20	1			3	7	
Insurance	19	3			1	8	
Dentist	18	2			6	7	
Doctor	17	2			7	7	
Veterinarian	18				7	7	
Coffee (Café)	17	2			20	8	
Bagels	17	1			18	7	
Liquor	20	1			6	8	
Video/DVD	18	2			8	8	
Office Supplies	20				5	8	
Hardware	18	1			18	8	
Pharmacy	18	1			11	7	
Books	16				19	7	
Baked Goods	14	1			22	7	
Entertainment	18				6	8	1
TOTAL	541	149	26	0	252	274	34

Gourmet Food/Health Food

Tim Hortons

Ice Cream

Ice Cream shoppe

Ice Cream shop

Card/Hallmark store.

Pizza

Pet care/grooming.

Need Olive Garden. More restaurants/Starbucks.

Hamlet of Egypt Economic Development Survey

Additional Comments

This area has only one restaurant we frequent - Applebees. We go to Henrietta to eat out.

Essentially none of the above exist in Egypt.

Sidewalks on Route 31 and a safe and easy way to cross 31 to use the park next to Lollipop Farm.

Re: Hardware - currently go to Home Depot.

Re: Gas - BJ's is cheaper

Re: Electronics - Currently go to Henrietta. Definitely.

Re: Groceries - small milk/bread type

Re: Restaurants - only Applebee's otherwise go to Henrietta. More variety.

Hamlet of Egypt Economic Development Survey

5) Where do you perform most of your total shopping expenditures? (check one)

Hamlet of Egypt
 Village of Fairport
 Perinton Square Area
 Eastview Mall Area

	* rarely, if ever, shop these areas
1	* rarely, if ever, shop these areas
38	* rarely, if ever, shop these areas
24	

Other (type in) *Perinton Square Area: groceries, banking, drug store; Eastview Mall Area: clothing, crafts, all other*
Wegmans, Eastview Mall, BJ's, Target, Home Depot, Best Buy (Henriette), Borders & Circuit City - in that order
Wegmans
Catalogs.
Wegmans Plaza.
Wegmans
Tops/Wegmans
(none selected) Wegmans on 31.
Eastview, Internet
Wegmans - Perinton
Wegmans

Hamlet of Egypt Economic Development Survey

6. Based upon the shopping location that you selected above, please rank the following reasons (from 1 = most important reason to 10 = least important) why you purchase most of your items at this location (bold the number)

Ranking

Reason for Preference:	Most Important							Least Important		
	1	2	3	4	5	6	7	8	9	10
Proximity	26	4	6	3	7			1	1	3
Convenience	23	10	4	3	7	1	2		1	1
Prices	12	3	5	3	8	5	3	3	1	2
# of Choices	20	4	8	2	5	4			1	3
Uniqe Items	8	2	3	1	7	1	5	13	1	6
Quality	19	9	5	3	4	2	2	2		2
Atmosphere	11	5	4	5	8	4	3	3	1	2
Familiarity	13	6	5	2	10	1	1	3	2	2
Customer Srvs.	12	4	9	2	5	1	2	4	5	2
Other	1						1			2

(write in other reason)

Most activities are east of Egypt - consequently the proximity of conveniences are used since few are offered / Ability to do one stop shopping.

I can purchase everything from groceries, take-out food, toys, gifts, liquor, and movie rentals in one location pl eat out and perform banking. For clothing and items for the home as well as discount shopping, crafts, and movie going, Eastview is a few minutes away.

It's there. I would travel further if need be.

Most of my shopping is done at Wegman's, cleaners at 250 & 31. Gas at Hess on 31.

Multiple stores, indoors, competitive prices. Can do lots of shopping at one time and its convenient.

(none checked) - Easy to go from one store to another better able to handle traffic flow.

Because the area fills needs (grocery, gas) I have and some wants (DVDs, bagels, restaurants, gifts, person services).

Feeling of community; I always see and speak with those I am familiar with; familiarity with merchant staff.

We are intent on supporting Egypt business when possible.

I really like shopping close to home, I like giving my business to local people. We contracted with Renovator because they are in Egypt, local people helping local people. Networking has been advantageous as various neighbors are now interested in using these remodeling experts. I miss Netzme--.

Hamlet of Egypt Economic Development Survey

7) Please indicate the number of people living in your household in the following age categories (type in the number of persons in each category) and indicate your age by checking the category matching

	Based Upon 58 Responses:	
0-10 years	42	21%
11-19 years	26	13%
20-30 years	6	3%
31-45 years	36	18%
46-60 years	59	29%
61 + years	34	17%
	203	

Please complete the following sentence: "I would shop more in the Hamlet of Egypt if:

... there were more shops catering to crafts, toys, books.

... I do not want to shop in Egypt.

... I didn't live near the shopping area and Rt. 31 was not there.

... small, convenience-type shops - i.e. coffee, bakery, basic groceries - NO big box.

I wouldn't like to.

... there was a (gourmet - health food - competitively priced full service wine) shop - a single store.

... there was a more varied selection of stores and services and the traffic situation were resolved. It's too hard to get into and out of the establishments that are there.

... there was a cleaners, bakery, etc.

...there were the type of stores I'd like to see encouraged in Egypt; however, I am not interested in "shopping" in Egypt.

... there was a variety of stores.

... it was safer and easier to pull out onto Route 31.

... they had more to offer and traffic was managed better.

... there were stores that sold things we wanted to buy.

... there were more variety of goods and services.

... family-oriented businesses were present - ice cream shop bagel/donut shop, toy store.

... the shops offered unique items - they can't compete with the large stores

... there were more unique, unusual shops.

... there were stores selling the types of merchandise I purchase regularly.

... not interested in more shopping in the Hamlet of Egypt. Prefer to keep it small and low key and tasteful.

... more were available to fit my needs.

Hamlet of Egypt Economic Development Survey

8) What changes would encourage you to shop more frequently in Egypt e.g. types of stores, pedestrian and vehicular access?

Egypt will never be Schoen Place and we don't want it to be!!!

My church (Resurrection) on Mason - I can walk. The fire house - makes me feel safe. Location - convenience to expressway, Per. Square & Eastview.

More variety - coffee/bakery/groceries. Sidewalks!, more parking but not over populated.

Types of stores, pedestrian access.

Continuous sidewalks, coffee shop (esp. with drive-thru), donuts, bagels, diner for family type meals, breakfasts.

Stores I shop at have what I want at the best available price. Small shops do not offer that.

Grocery store.

Types of stores listed in #4 above. Safe access for vehicles, bikas & pedestrians.

None - I really don't want Egypt to become a shopping area - we have sufficient places in the nearby area.

None. Contemplated Route 31 changes will keep it a dangerous road which further dampens any interest in shopping in area. Any interest is in driving through as quickly as possible without getting hit or rear-ended.

No changes would, I like to shop in the malls and large stores, and do not want those close to where we live.

Types of stores

Better variety of stores.

I shop at the local Wegmans, I buy my gas at the Mobil on PP Rd. 'I don't want more large shopping areas built.

More sidewalks. A traffic light at Mason. Easier access when turning left onto 31.

Currently, there is virtually no shopping in the Hamlet of Egypt, but every convenience and necessity is available between Perinton Square and Eastview. There is plenty of vacant space, plus vacancies up the street on 31 (Courtney Commons, Cross Keys, etc.). Traffic is fast-paced, and the Loud Rd. intersection is dangerous. It is very difficult to cross 31 to get to the bike path.

More selection of available items.

None.

**None. There would have to be a major shift in the cultural mindset for people in Egypt (or virtually anywhere in this country) to start walking to shop; if people don't do it (much) in the Village, they're not likely to do it along a well-trafficked - and let's face it - less appealing stretch of road & buildings set far apart. Can the Town provide incentives for current businesses to do facelifts on their buildings?*

None.

Quality clothes boutique.

Eastview Mall complex, Chase Pitkin.

There would need to be something unique about it since Perinton Square has just about every convenience. Traffic is very swift, and it is hard to get in and out of existing businesses. No one would walk to these stores. I would like to see all the vacant buildings filled before further land is developed. I would like to see a cohesive architectural look to these and any new buildings in order for Egypt to have any identity.

Vehicular access.

Can't really say because the Village of Fairport has the specialty shops that I occasionally shop at. Seems like Egypt would parallel or duplicate what Fairport has to offer so if they did I would probably spend my time visiting either of them.

Friendlier traffic flow.

I would shop more frequently in Egypt if the merchants, stores, etc. were of more interest to me. (I have no interest in tanning bed or tavern).

Pedestrian walkway and vehicular access and exits. It is far too difficult to exit those shopping areas onto route 31. More traffic lights are not the answer. Good design is.

Small, quaint shops that would encourage walking around town like an ice cream parlor and a gift shop like Port of Pittsford. A parking lot in back shops to the east side of the Hess gas station and removal of the big warehouse on the corner or at least making it presentable. I think the fitness center that is on the corner should stay there but get rid of that dilapidated building and put in a nice, SAFE one. I wouldn't even THINK of going there because I'd be afraid of what could happen to the building at any given time. I really wish we could get some sports fields in all that open space (on the north-east side) as there are NEVER enough fields for the kids and it would be a positive environment for them! This would also bring people into the area to shop at the little stores.

Different variety of services, stores, etc. of quality.

I do not anticipate shopping in Egypt.

The eyesore at the end of Mason Road has to go. That building was a mess 20 years ago. Buildings need to come down, make the hamlet look more presentable. We don't need more gas stations, more fast food _____. We need something for children, like a Gymboree, a pet store, even a car wash would be preferable to what is there now.

More family oriented stores, eg. family restaurants, toy shop, coffee shop, ice cream bar. Easy walking links between shopping areas and the system - this is one reason we like Fairport, because you can park and then walk the canal and get an ice cream!

Hamlet of Egypt Economic Development Survey

9) Name three features you like best about the Hamlet of Egypt and why?

Existing housing - nothing less

(1) There are hills, valleys, foliage; (2) People are friendly, helpful, interesting; (3) Location is far enough from noise but close enough to services.

Light installed at R. 31 & Mason. You literally take your life in your hands when making a left hand turn.

Close to stores yet also close to trails/wilderness/nature.

Rural feel, open space, historic buildings.

(1) Schools and proximity to places we shop at. (2) Good safe neighborhood and surrounding. (3) Neighborhoods of community oriented people.

Not a densely populated area.

(1) Sense of community. (2) Familiar - others with children. (3) Location - close to employment, shopping, services.

(1) Rural atmosphere - used to be quiet. (2) Not a speed trap - especially on Mason Road.

(1) Historic past. (2) Rural feel. (3) Trolley bed bike patch (except for dangerous crossing at Rt. 31).

(1) Its rural atmosphere (if we can keep it that way). (2) The open spaces - I don't like neighbors within an arm's reach.

(1) Lack of retail. It is not like West Henrietta or West Ridge Road. (2) MacGregors. (3) Hess gas station.

(1) The Suburban setting. (2) The convenience to major stores, Rochester, and work. (3) The parks, trails and open areas.

(1) Proximity to home. (2) Proximity to shopping. (3) Proximity to work.

Do not shop often to comment.

(1) Its quaintness, how traffic density off the main roads, family atmosphere.

(1) Country atmosphere - close to the city. (2) Large mall just minutes away. (3) No neighbors across the street - quieter.

(1) It's a quiet bedroom community. (2) It has an open country appeal with historic significance. (3) It has a great bike trail, although traffic poses a challenge.

Nature-wildlife in Egypt Hills/Lack of traffic (Egypt Hills)/Amount of undeveloped land.

Friendly. Still somewhat rural. Handy to Rochester - Village of Fairport.

Town pickup.

Low density, suburban residential, rural feel - my family wanted to be close enough to commute to the city but far enough away to feel outside it. If we'd wanted higher density and excellent schools, we would have bought in Brighton. We lived for many years in the city of Rochester and deliberately chose this area for its limited development.

Quaint, quiet, rural.

Not too commercial.

Not having big commercial complex. Having 250/31 complex is good enough.

Suburban - atmosphere. Convenience. Its home.

Open spaces - I like the rural feeling and being close to nature. Single family homes - not so populated. Not really near large populations of people like apartments.

Quiet, family atmosphere. Historic significance. Proximity to conveniences.

Small, informal, character, low-key.

Lack of development. Mainly housing area.

Small/commercial.

It's not a destination. It's a "through-street", of sorts, and people don't stop to shop.

Gas/diesel fuel availability, options for take-out food. Auto repair/tire purchases - all convenient and product service is good.

Rural atmosphere, close by major shopping areas.

• Country feel while close to Fairport & Eastview. • Good, caring neighbors • Town of Perinton - I lived near Baptist Nursing Home before ...

I identify with the Perinton/Fairport area. I don't identify with "Egypt". I like the Middle/upscale living ambiance, the convenience to Eastview Mall, and the easy access to major highways.

The historic background, the desire of the citizens to maintain and preserve the beauty, the availability of natural enjoyment (trails, parks, etc.)

Quiet, not over populated.

Quantity of open space, neighbors, wildlife.

Pleasant atmosphere. Not built up with strip malls.

Convenience.

Egypt is a vestige of the stagecoach era. It has lost its historic character. Automobile transport dominates in suburbia. Egypt is not friendly to shopping via automobile due to off/on access from a relatively high speed road, lack of large parking lots, and a wide variety of shops.

-Historical atmosphere. -Well-maintained homes, both older and new. -Sense of neighborhood.

It is nearby.

(1) Rural character. (2) Convenience to local shopping areas. (3) Historic nature and charm.

1) Away from the business community. 2) Close to the High School. 3) Close to a park but only wish access was easier without driving.

Convenience, or as realtors say Location, location, location!

"Rural feel"; openness of the Egypt Hills; good neighbors.

Hamlet of Egypt Economic Development Survey

10) Name one thing that you would like to see changed in the Hamlet of Egypt and why?

Most housing areas are lovely, parks & Lollipop beautiful.

1. The possibilities are great, since it's already commercial, in making it a quaint little town. 2. It is very close to where I live (and right now I think it's a waste of very good land). 3. It has the potential to become a real community that we can be proud of and be part of right here in our front yards.

It's size, it's scenery, neighborhood.

Quality of life. Outskirts of city. Convenience/proximity to necessities of life.

1. Little transience (people remain in neighborhood, allowing sustained friendships). 2. Rural atmosphere, allowing quiet, privacy & abundant wildlife. 3. Convenient to shopping, work, recreation.

Small low key and not a mall! Like the tiny hamlet feel of it. Do not want it enlarged.

Historical nature - Becoming an attractive entrance to our neighborhood.

Small, rural in nature, relatively low key.

Dick Keenan has done wonders for this hamlet, I'm just not in a hurry to visit him. The firehouse looks very appealing and I really love the Mason Valley neighborhood the way it is right now!!!!

1. Open-ness/semi rural feel - I (currently) love the view from my back yard. 2. Convenient for 250/31 shopping and East View Mall. 3. Family neighborhood - quiet cul de sacs which are great for small children.

Mason/Loud Warehouse? - Gone

Broken/dilapidated house on corner? - Gone

East & west corners of Loud Rd - it's a disgrace to Egypt to have to put up with one or two property owners who care nothing for appearance of his property.

Sidewalks.

Elimination of old Nelson store and Comstock building.

Better pedestrian access with connections to bike paths.

(1) Move landscaping business east to undeveloped land and put well planned and beautiful development of single family homes between \$150 and 250 thousand. No homes currently available in that range. (2) Add quality restaurants (chains OK), low key family entertainment. (3) Ease traffic congestion or don't increase it.

Should be preserved - not allowed to be too built up.

Former Comstock plant removed or fixed up - it is a disgrace.

Bulldoze the Cannery building - it's an eyesore.

I would like it cleaned up (e.g. the former canning factory and store on corner). I cringe every time I drive by those places. The factory was better when it was a factory - at least there was an attempt to keep it clean.

The Comstock plant and the "historical house" at the Route 31/Loud Road interchange. The Town should be more concerned with cleaning up the hamlet first and creating a unified look before embarking on more disjointed growth. Citizens/taxpayers should determine what how and when the hamlet gets developed and not politicians, appointed or elected officials.

Clean up the warehouse and old general store on the corner of Rt. 31 and Loud Road.

We are disappointed by the push to re-zone land in the Hamlet of Egypt. We feel that current re-zoning plans are making us reconsider our permanent residency in the Hamlet of Egypt. It is a misuse of power for town officials to rezone land to accommodate an aggressive developer. Re-zoning will directly affect our perception of the quality of life, security, and re-sale value of our home in the Hamlet of Egypt. Frankly, we would not have purchased our home on Mason Rd. in 1998 if we were aware of the possibility of residential "B" housing built on the Ranny property.

Do not believe makes sense to further develop this area, there is enough development in close proximity to

More preserved area's, NO CAR WASHES!!! Single family homes ONLY! There is no ownership in apartments. Keep it the way it is. If building will take place, then add small shops in one geographic location similar to a seaside resort type setting. Pedestrian traffic, brick style walkways small antique and craft type stores.

More radar set up on residential streets - people driving 45 MPH and in a 30 MPH zone at all times.

I would like to see alternate uses of vacant buildings before any building is approved. I would also like to see the bike trail have safer access.

Less traffic on 31/No more traffic on Thayer/Bluhm.

Route 31 developed for "village type" use; that is grocery store, hardware store and this type of businesses. So we can shop more locally without having to drive.

Less building - more open land.

Nothing more than a careful evaluation of the supposition that higher density residential development on the remaining land in Egypt will somehow transform the economic viability of existing (or even future) businesses along the Rte. 31 corridor - this when existing residential developments haven't seemed to have done so as they've been added over the years. Are apt. dwellers really any more likely than everyone else to walk to shopping? (*see #8).

N/A.

Slow down in population growth because of the increase in traffic on Route 31.

Improve traffic flow - new school will make it more difficult to enter Rt. 31.

Renovate corner - south side of 31 and Loud Rd.

Tear down the Cannery - it is an eye sore. I would love to see a park or garden in it's place.

I would like to see traffic made safer, particularly at Loud Rd. and 31, and at the entrance to the bike trail.

Hamlet of Egypt Economic Development Survey

10) Name one thing that you would like to see changed in the Hamlet of Egypt and why? - cont'd

More walking opportunities on Rt. 31.

Traffic light at Loud and 31.

Get rid of the old canning facility. I still prefer an old solution to the Rte. 31 access problem. This would give Egypt the chance to develop as a small hamlet rather than a main road strip. (see diagram).

Better traffic control to manage the volume of traffic. Stop light at Loud.

Clean up Comstock (former property) eye-sore - detracts from bring in other business, a failure of the Planning Board to enact controls over develop of Comstock property. Get rid of or cleanup the "slum lord" property appearance.

Cleanup or tear down the "eye sore" buildings.

Do something about the Comstock Building - It's a disgrace to the neighborhood.

Get rid of the Comstock eyesore, tear down that dilapidated building across from the Egypt Firehouse (eyesore) and most importantly get a traffic light at the corner of Mason Road and Rte 31 to eliminate a safety hazard.

Create a special atmosphere in the area - more as a place for enjoyment than shopping per se, keeping it small and confined to the Rt 31 area.

There isn't really anything I would change. I love the area.

The unsafe conditions when trying to get out onto Route 31 and the length of time required when doing so.

If more shopping is added, it should be off the road - not strip mall style.

Traffic and vehicle flow needs major improvement.

Some historic lighting along the highway would be helpful to remind folks of the historical significance of Egypt, just as the banners portray now.

Warehouse/ex-canning factory removed.

Get rid of the Comstock building - derelict building, ugly, trailer-truck traffic (Loud & 31).

Better and safer access onto Rt. 31 from side streets.

Make the community more family oriented with walkways, benches bicycle paths and trees.

Tear down or do major rehab on old Comstock plant. It's an eyesore.

Cleaning up intersection of Loud Rd/Rt. 31 - traffic light installed, get rid of warehouse and the old store on the corner to make the area more attractive and a place where people would like to see or visit.

Business district needs to be cleaned up - LOOKS TACKY - with some consistent character (styles? don't mesh).

If that old building on the corner of 31 and Loud Road is so important to the Historical Society, then why hasn't it been moved or fixed? It is getting more and more damaged as they continue to let it rot. I'm sure it's a health hazard and I also thought they said they wanted to take it somewhere to restore it. If that's the case, then they should move it! It would even be ok if they leave it in the Hamlet and restore it! If it's money (I'm sure that has a lot to do with it) then let's come up with something to raise the money! Have a car wash - bake sale - craft sale fun day in the fire station parking lot or something to get it started! I'm sure the Sears building parking lot is too dangerous to have it there. **My one thing to be changed: Cleaning up the corner of 31 and Loud Road (the little store and the building and parking lot across from it) would be an EXCELLENT start to making it a better looking place!**

Renovation or demolition of numerous run down properties will definitely add to the quaintness and desirability of area.

I would like to see an enforcement of speed limits, a "village" type atmosphere with shop-front consistency (like Bushnell's Basin), and promotion/enhancement of historically significant structures.

Without enlarging, tidy/spruce up any dilapidated existing buildings with sensitivity to historic elements and quality components.

Former Comstock factory!! Depresses the hamlet visually and economically.

Tear down the warehouse at the corner of Loud and 31. It is a detestable eyesore.

The eyesores.

Route 31 façade - upgrade Comstock, and Egypt Plaza. Visually identify the Hamlet better - by speed reduction on a state road (like Bushnell's Basin) and good street lighting, etc.

Additional Overall Comments:

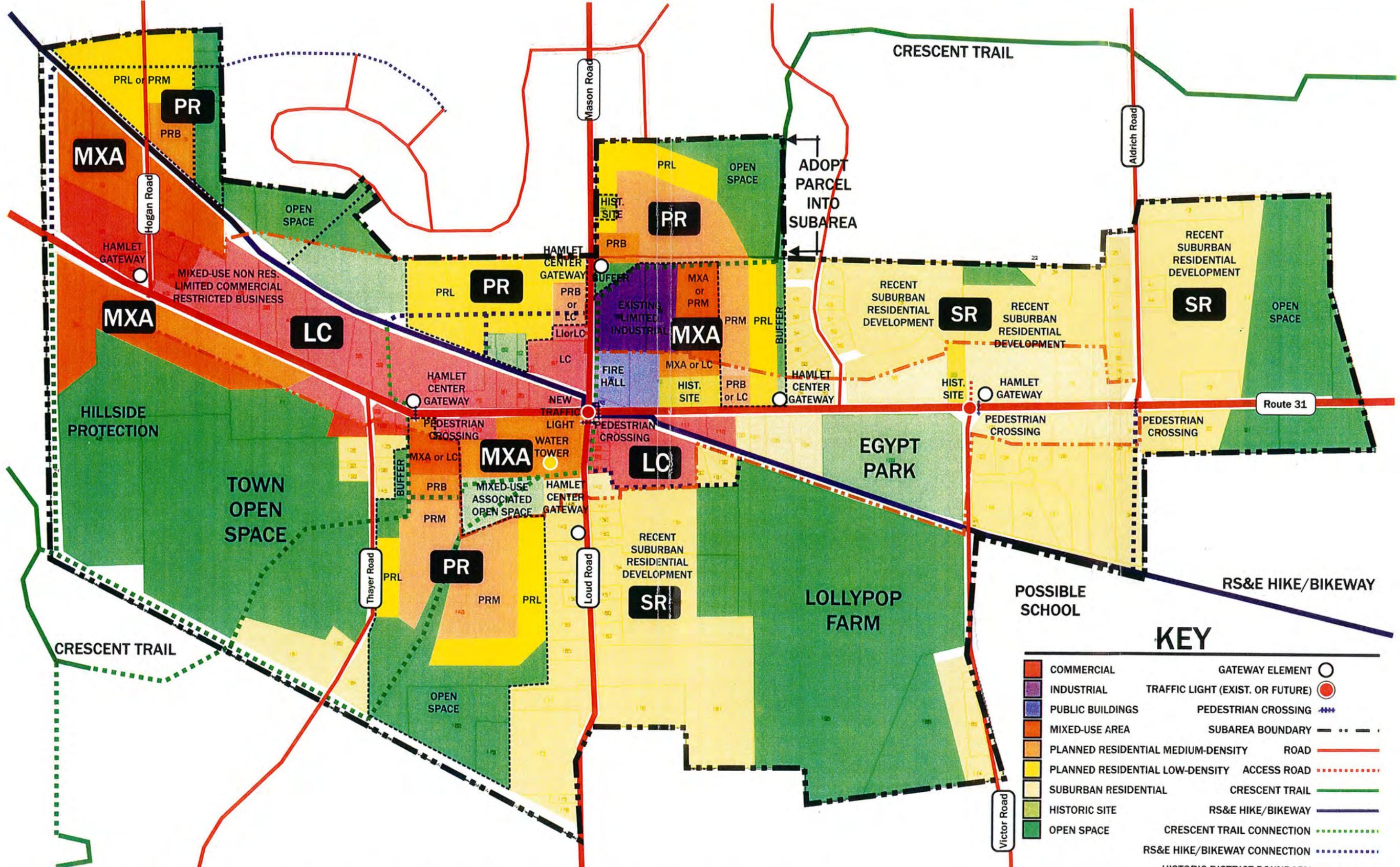
Town of "Perinton, NY" post office would be great!

My family does NOT want Egypt developed. We want a traffic system that handles the volume swiftly and safely. Making Egypt a "destination" center is a joke.

I'd like to see businesses, such as the following, in Egypt: ● an ice cream store (like Abbott's); ● an antiques store; ● a gift shop; ● a clothing consignment store (upscale - like Anything Goes in Fairport Village); ● some additional casual restaurants - maybe Italian, Greek, Thai - like Mamasan's - not sports bars! & not chains.

** ● a crafts/antiques coop/complex like the one located in Greece (that used to be in Hilton). An ideal spot for it would be the Comstock Building. Maybe someone could approach the owner with this idea? This could potentially pull in customers from all of the surrounding towns, making Egypt a destination, instead of a place to pass through.

Egypt is a nice little hamlet. I would like to see it kept that way. We have enough nearby places to shop for common items (Wegmans, Eastview Mall). I would like to see Egypt have unusual, unique shops to make Egypt an adventure to shop in.



THE HAMLET OF EGYPT SUBAREA PLAN 2003 LAND USE & CIRCULATION

HAMLET OF EGYPT TOWN OF PERINTON NEW YORK

